

Trail Town



CITY OF LA CAÑADA FLINTRIDGE



TRAILS MASTER PLAN

March 2006

USA



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Society



The City of La Cañada Flintridge was declared a "Trail Town U.S.A." in 1996

DISCLAIMER

Pursuant to California Government Code 831.4, maintenance and/or design standards set forth in the City's Municipal Code, Trails Master Plan, or other City ordinances or documents shall not be construed as a standard of care for any Trail unless otherwise expressly prescribed by state law.

TRAILS MASTER PLAN

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CITY OF LA CAÑADA FLINTRIDGE TRAILS MASTER PLAN

INTRODUCTION

Trails have been an asset of La Cañada Flintridge since the very early days when the community was settled. A real estate advertising map from the 1920s shows the bridle paths of Flintridge as an important amenity of this new community (Appendix 2).

When the two communities of La Cañada and Flintridge incorporated as the City of La Cañada Flintridge in 1976, the trails were subsequently included in the General Plan. References to the trails can be found in the Land Use, Circulation, and Environmental Resources Management Elements (see Appendix 9). There is, however, no single, comprehensive reference for trail planning in the City. In April 2002, a Mayor's Ad Hoc Trails Committee was appointed to create a Trails Master Plan. This is that plan.

PURPOSE

The Trails Master Plan is a statement of the City's long-term commitment to trail protection, improvement and expansion of trails. The purpose of the Trails Master Plan is to provide a detailed guide as to how the goals and policies of the General Plan relating to trails are to be implemented. The Trails Master Plan is intended to be the implementation document for the General Plan regarding trails and trail-related issues, upon the next update of the General Plan.

The Trails Master Plan is a blueprint for the City to follow for the continued enhancement and protection of the trails. Specifically, it will:

- Catalogue and map the trails within the City;
- More fully describe and identify our existing trail resources, including some of the trail easements along existing trails;
- Identify all properties adjoining or underlying the trails;
- Identify potentially threatened or lost trail segments;
- Establish procedures for reporting trail encroachment and maintenance problems and issues;
- Ensure that all development on parcels affecting the trails is carried out in a manner which assures the protection of the trails system, including the development of a uniform procedure for review by the Community Development Department when a property undergoes staff review due to development or other reasons (such as code compliance);
- Establish design standards or guidelines, including trail widths, signage, street crossings, drainage, etc.;
- Identify potential locations for future trail development and connectivity, including “missing links” in the trail system – and identify potential funding options and volunteer improvement programs;

- Coordinate planning and standards with other agencies, such as Los Angeles County, regional parks districts, open-space districts, state parks, and national parks and forests; and
- Establish policies for trail accessibility consistent with applicable law.

The Trails Master Plan, including the Trails Map, should be considered a "living document" that may be periodically revised to enhance and protect the trails. To accomplish the purpose as listed herein, portions of the Trails Master Plan, especially the maps, will require updating on as needed.

GOALS AND OBJECTIVES

The following goals and objectives represent the guiding direction for the planning of the trails system:

The City of La Cañada Flintridge Trails Master Plan:

1. Will provide that the trails shall be multi-modal, non-motorized recreational trails and shall be designated primarily as a soft surface trail system, providing recreational opportunities for hikers, bicyclists, and equestrians, while restricting motorized vehicles from trails.
2. Will provide for a loop trail system, providing a circular trail system which circumnavigates the bulk of the city, enhancing the trail experience for families, hikers, bikers, equestrians and those using the trails for non-motorized transportation.
3. Will protect the integrity of the existing trails and trail access points so that the trail network does not become broken and inaccessible to trail users.
4. Will provide for diversity in trail experiences including variety in difficulty, terrain, environment and the exploration of interpretive opportunities.
5. Will link recreational opportunities within the City of La Cañada Flintridge providing for signage to and connections between trails and activity centers within the City as well as to trail systems in adjacent jurisdictions.
6. Will be designed with safety as a paramount concern and will minimize vehicular crossings; maximize the separation between roads and trails (with plantings and distance); maximize visibility, warning signage, and in-pavement flashing crosswalks with warning flashing lights where appropriate (in those cases where interface between vehicles and the trail does occur); and provide access for patrol, emergency and utility vehicles and maintenance where possible.
7. Will address issues of regular upkeep to ensure trail safety.
8. Will provide a signage system of easily recognized uniform trail markers which will allow users to clearly recognize the trail system.
9. Will, whenever possible, designate trails in locations that will provide the community with trails, yet minimize the financial burden to the City of La Cañada Flintridge by (a) establishing

policies for the dedication of trail areas (or payment of in-lieu fees) in conjunction with new development, including standards for the amount and quality of land required, consistent with applicable law;. (b) locating trails along existing paths, roads and utility easements; and (c) minimizing the need for property acquisition.

10. Will establish trails which will minimize impacts to all affected parties to the degree possible.
11. Will locate trails with environmental sensitivity.
12. Will integrate a City-wide system of bike lanes with the trail network to the extent possible to encourage non-vehicular circulation within the City.
13. Will develop the trails to maintain the semi-rural appearance and character of La Cañada Flintridge.

TRAILS IN LA CAÑADA FLINTRIDGE

Trails have been a part of the culture of the City since it was settled. The equestrian heritage is strong today. Many residents keep horses on their property and have trail access to the Flintridge Riding Club, established in 1923. The presence of horses and riders enhance the semi-rural character of the City.

The La Cañada Flintridge Trails Council, along with many other local residents, has continued to work with the City to maintain, repair and expand the trail system. In 1996, the Trails Council submitted a comprehensive proposal to become one of the few cities chosen to receive a special designation for its trails. The City of La Cañada Flintridge was named “Trail Town, USA” by the American Hiking Society, based in Washington, D.C., in recognition of the community’s trail history and commitment to those trails. The City competed with many cities for the honor, and was chosen one of the ten top trail towns in the United States. This is indicative of the importance that trails play within the City.

TRAILS MASTER PLAN AUTHORITY

The Trails Master Plan will ultimately become the enforcement or zoning document of the General Plan when the General Plan is next updated. At that point, the Plan will have the authority given to it by law.

The authority for preparation and implementation of the General Plan is in State law. State law requires that each city adopt a comprehensive plan for the physical development of the city. Although having a separate Trails Element is not specifically mandated by state law, Government Code Section 65560(b)(3) stipulates that the General Plan address “...open space for outdoor recreation including, but not limited to ...areas which serve as links between major recreation and open space reservation, including utility easements, banks of rivers and streams, trails, and scenic highway corridors.”

The Trails Master Plan will be the enforcement tool of the General Plan once the Trails Master Plan is formally adopted through the public hearing process. At that time, it will have the force and effect of Planning Law. Land use proposals involving trails will have to comply with the Trails Master Plan.

EXISTING AND LOCAL REGIONAL PLANS

Local Plans. The Trails Master Plan incorporates information included in plans and documents that the City has already adopted. These include:

- City of La Cañada Flintridge Annual Budget and Financial Plan - FY2005-06
- City of La Cañada Flintridge General Plan

Regional Plans. Regional planning efforts and trail plans prepared for adjacent cities and areas have also been considered in the Trails Master Plan. These include:

- City of Pasadena Arroyo Seco Master Plan
- City of Glendale General Plan
- County of Los Angeles General Plan
- County of Los Angeles Master Plan for Trails (Map) (Appendix 4)
- County of Los Angeles Trails Guidelines
- City of Pasadena General Plan
- Transportation Improvement Program (MTA)
- Business Plan for the Angeles National Forest
- Santa Monica Mountains Comprehensive Plan
- Rim of the Valley Trail Corridor Master Plan
- Open Space Plan for the Los Angeles & San Gabriel Rivers

ACTIVITY CENTERS

The identification of “activity centers” in La Cañada Flintridge is important to determining trail locations. An “activity center” is defined as any place that can attract trails users, including recreational, educational, and employment centers. These activity centers are shown on the Activity Centers Map included as Appendix 13. Existing activity centers include the following:

Commercial Centers. La Cañada Flintridge provides many shopping opportunities to area residents. The City has several small shopping centers as well as many other local businesses, primarily along Foothill Boulevard.

Community Center. The Roger Barkley Community Center is owned by the City and operated by a non-profit organization. It is a primary focal point for community recreational activities.

Y.M.C.A. The Crescenta- Cañada Y.M.C.A. is a large recreational facility located within the City that serves the residents as well as the regional community. This facility is a major focus of recreational activity.

City Hall. City Hall, also on Foothill Boulevard, is a hub of community activity.

Descanso Gardens. Descanso Gardens is another focal point of the community. It is a large botanical garden, hosting thousands of visitors each year. It is well-known for its flowers and other natural attractions and offers educational experiences for the public. Additionally, it is famous for the many weddings it hosts on its beautiful grounds.

Flintridge Riding Club.

There is much community activity at the Flintridge Riding Club. Established in 1922, the Flintridge Riding Club, a 40-acre facility, located just off Oak Grove Drive and north of Foothill Boulevard, provides stables and paddocks for up to 180 horses. It is a private club with training, equestrian shows and social activities in connection with equestrian activities.

Jet Propulsion Laboratories. Jet Propulsion Laboratory, located on Oak Grove Drive employs approximately 6,000 employees, many of whom live within the City. The transportation activity alone generated by this facility is substantial.

Open Space. The General Plan states that there are 938 acres of private parks, public open space, private open space, trails and community recreation club areas within the City of La Cañada Flintridge. This comprises approximately 17% of the City. The total City acreage is listed as 5,525.

A large amount of open space is owned by the City. Much of this open space is Cherry Canyon in the San Rafael Mountains, along the southern edge of the City. This canyon was acquired by the City in several different parcels over a fifteen-year period of time from 1985-2000. There are a total of 6.83 miles of trails and fire roads in the Canyon. Of this, 3.71 are trails owned, operated and maintained by the City.

The City acquired Hall-Beckley Canyon in 2003, a five-acre open space area that is a portal to the Angeles National Forest. A trail extends through the City-owned land and connects with other local and regional trails in the mountains to the north of the City.

The City also owns the Rockridge Park and Conservation Area. This area is to be developed as a natural open space area with trails and educational opportunities. It is located close to the Crescenta-Cañada YMCA complex.

The County of Los Angeles also owns approximately 361 acres of open space in the City. The Federal Government owns a 44-acre parcel within the City, next to the Angeles National Forest, but not a part of it. The remainder is privately held.

Appendix 5 is the City Zoning Map, displaying the areas of open space within the City.

Rose Bowl Riders. Although outside the City, much equestrian activity can be found at the Rose Bowl Riders. Established in 1946, the non-profit, volunteer-supported equestrian club sits on approximately twelve acres in lower Hahamongna Watershed Park off of Oak Grove Drive. The land is leased to the Rose Bowl Riders from the City of Pasadena. The boarding, training, show and recreational facility can house approximately 100 horses.

La Canada Flintridge Country Club. Situated off Angeles Crest Drive on the north side of the City, this property has, for its members, a golf course, a 25,000 square foot clubhouse, six lighted tennis courts and an Olympic size pool. It was built in 1962 and is situated on approximately 100 acres. It is a key activity center in the city.

Utility Corridors. Edison has both fee-owned property and easements in La Canada Flintridge (approximately 86 acres) which are improved with electrical (transmission and/or distribution) and communication facilities. SCE has licensed a portion of these properties for trail use to the County of Los Angeles, and is in the process of licensing additional properties to the City of La Canada Flintridge for trail use for the Loop Trail.

Parks. The City of La Cañada Flintridge owns and operates four formal parks. The parks are: Memorial Park, Mayors' Discovery Park, Glenola Park and Glenhaven Park. Through a Joint Use Agreement with the Church of Jesus Christ of the Latter Day Saints, a sports field owned by that church which is adjacent to Mayor's Discovery Park is also utilized by residents and maintained by the City.

Schools. There a total of 30 Schools (public and private, including preschools) in the City. Currently, nearly 4,500 children attend La Cañada public schools alone.

Schools in La Cañada Flintridge		
Level	Public	Private
<i>Pre-School</i>	0	11
<i>K-3</i>	0	1
<i>K-6</i>	3	1
<i>K-8</i>	0	1
<i>K-12</i>	0	6
<i>4-8</i>	0	1
<i>7-8</i>	1	2
<i>9-12</i>	1	2
TOTAL:	5	25

Although one-sixth of the community is served by the Glendale School District, none of these schools are located within the City.

The City of La Cañada Flintridge and the La Cañada Unified School District have an extensive Joint Use Agreement that provides for public use of certain school properties. This Agreement covers seven school fields, six tennis courts, a skate park, basketball courts, an auditorium and a library.

SETTING AND EXISTING CONDITIONS

La Cañada Flintridge is located approximately 12 miles north of downtown Los Angeles. It is at the westernmost edge of the San Gabriel Valley. The City is located in a spectacular natural setting with the San Gabriel Mountains to the north, the San Rafael Hills to the south, and the Verdugo Mountains to the southwest.

In the 1920s, when the hills of Flintridge were subdivided, bridle paths were included in the plans for the community. In the mid-1930s, Los Angeles County developed an ambitious plan to create a comprehensive system of trails throughout the County. Most of the trails located within the City, with the exception of those in Cherry Canyon and certain other segments, are trails owned and maintained by Los Angeles County. In the mid-1970s, local trails were threatened by development as the town grew. The La Cañada Flintridge Trails Council was formed in 1973, as a nonprofit citizens' group to work with the City and the County to protect and promote the trails. Over the years, several segments of dedicated trails have fallen into disuse but, because of the town's devotion to the trails, the network remains largely intact. Since 1985, the City has, with the assistance of the Santa Monica Mountains Conservancy, and our State legislators, purchased large amounts of land in Cherry Canyon and constructed a number of trails in this park which are maintained by the City.

There are approximately 24 miles of trails within the City which wind through the town and surrounding foothills. These contribute greatly to the town's semi-rural atmosphere and quality of life. These are multi-use recreational trails open to pedestrian uses, hiking, equestrian, and bicycle use where feasible. They connect to an extensive network of trails in Pasadena, Glendale, South Pasadena, and Altadena, as well as the Angeles National Forest and are an integral part of a tremendous regional asset. It is possible to hike or ride from city trails to the Pacific Crest Trail which runs from Canada to Mexico.

Now, in the first years of the new millennium, the trails are once again threatened as property values rise and open space becomes ever more scarce. Trail maintenance is an issue of concern because of the budgetary difficulties faced by Los Angeles County. The trails benefit from an enthusiastic collaboration of residents and City government, both of which recognize the value of this unique community asset and are eager to build upon the work of those who have gone before. The Loop Trail, first envisioned in the early 1970s, and which is shown on the General Plan Map (Appendix 3) has moved closer to reality with the discovery of a utility flume over the 210 Freeway which will be used to connect the city's north and south trails on the west. The City has obtained grant funding to support building a trail over it and construction began in early 2006.

Trails maintained by the City (approximately 4 miles) are currently all on City-owned property, Edison property or Caltrans property. The County-maintained trails (approximately 20 miles) exist on County property, Edison property, federal property and privately owned property.

PHYSICAL CHARACTERISTICS

The City of La Cañada Flintridge encompasses approximately 8.5 square miles. The City is situated on the foothills of the San Gabriel Mountains, which form a visual backdrop. Approximately 800 acres of undeveloped land are on the slopes of these mountains, which are within the City's boundaries. A large percentage of this undeveloped land is held in public or semi-public ownership. Several canyons are in this portion of the community and it is lined with flood control channels and debris basins. These parcels have significant watershed, recreational and visual significance.

Natural rainfalls, which do not either drain through the flood control facilities or evaporate, help replenish and recharge the Raymond Basin, a ground water reservoir under the City of La Cañada Flintridge, as well as Pasadena. This basin furnishes a portion of the water used in the community.

The Sierra Madre fault passes directly through La Cañada Flintridge, but has not been designated as "active". The Raymond Fault is located nine miles away, and the San Andreas Fault is located 40 miles to the northeast.

The soil composition of the flatter terrain in the City consists of the Ramona loam and Hanford (stony) loam series. These soil types have high agricultural use value and can provide the base for citrus crops. Soils in the higher elevations of the City are composed of a Holland loam, which is a gritty soil usually giving way to its granite bedrock, and the rough, stony soil of granite origin. The steepness of the terrain and scouring action of water runoff has resulted in natural erosion. Following heavy rainfalls, large amounts of debris and topsoil are washed down from higher elevations. Although natural and introduced vegetation has substantially reduced the extent of local erosion, vacant land with steep slopes continues to erode significantly during moderate to heavy rainfall, having a detrimental effect on the City's trails.

Vegetation native to the community consists principally of chaparral of medium to high density on the San Rafael Hills and the San Gabriel Mountains. Many plant species comprise this chaparral including scrub oak, sagebrush, Spanish broom, manzanita, annual grasses, herbaceous shrubs and ceonothus. Trail users must exercise caution because of the proliferation of poison oak in the canyons.

The undeveloped open space vegetation is characteristic of a Mediterranean ecosystem which, according to the California Research Learning Center, is rare, occurring worldwide in only five areas and comprising only about 3% of the earth's total land area. The growing season for this native vegetation is in the winter months. Summers locally are characteristically long, hot and arid. Fires, if they should occur, burn off the vegetation periodically. Plants, shrubs, trees and grasses not native to the community are cultivated in residential gardens and other areas. These contribute significantly to the beauty and character of the local landscape.

The La Cañada Flintridge area is home for many species of small birds. Some commonly observed species include dove, quail, towhee, hummingbird, sparrow, jay, owl, wren, crow, raven, swallow, woodpecker, mocking bird and various species of finches. Larger varieties such as hawks, vultures and waterfowl can also be found. Squirrels, rabbits, rats, mice, gophers, chipmunks, deer,

mountain lions, bobcats, coyotes, skunk, raccoons, opossums, and various reptiles, such as rattlesnakes, gopher snakes and lizards can all be found in the community. Bees and other insects are abundant.

Trees have been a high priority in the community. The City has a tree ordinance (4.26 of the City of La Cañada Flintridge Municipal Code), protecting designated trees. Specifically, no native oak, sycamore, deodar cedar, Chinese elm or California pepper tree with a trunk measuring twelve (12) inches or more in diameter (as measured at a point four feet from the ground surface at the natural grade) shall be removed without a tree removal permit issued by the City.

Trees are abundant, and the City was awarded the title of “Tree City, USA®” by the National Arbor Foundation in cooperation with the USDA Forest Service and the National Association of Foresters in 1988.

Primary Source: Environmental Resources Management Element of the General Plan – City of La Cañada Flintridge

POPULATION CHARACTERISTICS

As of the 2000 census, the population of La Cañada Flintridge was 20,318. The City’s population remains stable, with little developable residential property.

RECREATION CHARACTERISTICS

The mild Mediterranean climate makes year round outdoor sports and trail usage possible. City parks and other major recreational characteristics are described in the “Activity Centers” portion of the Master Plan.

In addition to the many opportunities within the community, the City of La Cañada Flintridge is fortunate to border the Angeles National Forest, as well as the Arroyo Seco, both major recreational points of interest.

TRAILS INVENTORY

A complete inventory of all of the trails within the City of La Cañada Flintridge, with descriptions, location maps and photos can be found in Appendix 6. These trails are:

- Cerro Negro Trail
- Cherry Canyon Fire/Utility Roads
- Conservancy Trail
- Cross Town Trail
- Flint Canyon Trail
- Gateway Link Trail (proposed)
- Georgian Spur
- Gould Canyon Trail
- Hall Beckley Canyon/Earl Canyon Mtwy. Trail
- Horse Lane Trail

- Liz's Loop Trail
- Loop Trail Phase I (under construction)
- Loop Trail Phase II (proposed)
- Loop Trail Phase III (proposed)
- Lukens Connection Trail
- Owl Trail
- Rockridge Terrace Trail (proposed)
- San Rafael Hills Trail (proposed)
- Ultimate Destination Trail

These trails can be seen on the Trails Overview Map in Appendix 12.

Only existing trails that are fully anticipated to be built by the City are included in the inventory. Although there are other proposed trails, those were not included in the inventory due to the uncertainty of construction at the present time.

TRAIL OWNERSHIP

La Canada Flintridge's trail network, unlike that of many other communities, was not all neatly laid out beforehand and then systematically built. Rather, the trail network expanded out from the trails first created in the early 1900s. The trails have great character and diversity, varying in width from 10 feet to as wide as 15 to 25 feet. In some areas, the trail right-of-way is even wider, providing vistas and open green space which enrich the trail experience. The result is a trail network of greater complexity and less standardization than usual with a complex pattern of ownership.

The City-maintained trails in Cherry Canyon are also owned by the City. Many of the County-maintained trails are owned by Los Angeles County in fee. Of the County-owned segments, two Departments of the County of Los Angeles maintain trail segments in the City - the Los Angeles County Department of Parks and Recreation and the Los Angeles County Flood Control District. The City should continue to work with the County to ensure that no County-owned property within the City is sold to the detriment of the trail network. The County of Los Angeles is required to notify the City prior to the sale or vacation of County-owned property within the City pursuant to Government Code §65402.

The County of Los Angeles has had a consecutive series of license agreements with Edison since the 1950s for trail use on Edison property north of Foothill Boulevard. The license agreements also established the County's responsibility to maintain such trails and must be renewed every three years. The City has one license agreement and is in the process of negotiating a second agreement with Edison to use its property extending south to Descanso Drive for a trail.

Where the trails exist on property other than that owned by the City, County or Edison, recorded easements exist for most of the trails being used. There are a few areas where easements exist but either a trail has not yet been built in that location or the trail has fallen into disuse. There are a few areas where there is an existing historical trail, but no recorded easements are known. There

are also a few areas where there are existing recorded easements, but the trail appears to be misaligned due to physical constraints and located in an adjacent area.

Whenever possible, the City should acquire easements or other legal rights covering any existing trail where there is no known recorded easement (or where the trail is misaligned), as adjoining property is developed, permits are issued or other opportunities to clarify title arise. The City should continue to work with the County to investigate and resolve easement questions on County trails.

TRAILS - LAND USE APPROVALS

As a part of the development review process, City staff shall review the proposed development to determine whether such property is located adjacent to or within current trail use, existing trail easement(s) or proposed trail location(s). The Community Development Department Project Review Procedure: Trails (Appendix 7) shall be used by all City Planners in the review of projects and is subject to modification by the Community Development Director in keeping with the provisions of the Trails Master Plan. All trail impacts or potential impacts shall be considered as a part of the land use approval process and appropriate mitigation measures, if required and as permitted by applicable law, shall be incorporated into every project approval.

PROPOSED TRAILS ORDINANCE

To better protect the trails within the City, a new proposed Trails Ordinance will, if adopted by the City Council, be attached to the Trails Master Plan as Appendix 8. The ordinance will address trail issues such as dumping on trails, smoking/fires on trails, encroachment of trees/vegetation onto trails, drainage which damages trails, ingress/egress for private property owners on trails for construction, obstruction of trails, erection of fences along trails, use of vehicles on trails, littering, signage along trails, and consumption of alcohol on trails.

TRAIL ACCESS POINTS

Trailheads, Staging Areas and Entryways are all considered access points to the trails and are shown on the Trail User Map in Appendix 15. The purpose of this section is to define and identify those access points.

For purposes of this section, the following are the applicable definitions:

Staging Areas.

These are defined as a major entry to a trail with accompanying amenities, such as, water troughs, drinking fountains, directional and informational signs, formal parking areas, hitching posts, doggie waste stations, bicycle racks, decorative elements (such as split rail fences, plantings, and trail identifying features as appropriate), horse loading/unloading facilities and/or restrooms.

Trailheads.

These are defined as the beginning or end of a trail, or the beginning or end of a network of smaller trails, such as those in Cherry Canyon. Trailheads may have the same amenities with the exception of formal parking, restrooms, and horse loading/unloading facilities.

Entryways.

These are generally defined as all other trail access points, including spur trails which access the main trails and street access to the trails along their length. Entryways may have any of the amenities that trailheads have, as appropriate for the location.

Using the trails in the City involves knowledge of the access points. These entrances should be demarcated with distinctive markings and features so that the user not only has knowledge that the access point is there, but also information regarding the use of the trail, distances to other trail points, maps and other features useful for the trail user.

A successful recreational trail needs facilities to support the comprehensive system.

When developing future trailheads, the City should install distinctive trail markings and should consider whether it is physically possible to include other support facilities for the trail. Each access point will vary in terms of the facilities available.

Development of an existing trailhead is currently underway at the Hampstead Road entrance to Cherry Canyon. A staging area with a parking area for horse trailers is being planned for the Loop Trail Phase II at Foothill Boulevard and Indiana Avenue. The City plans to license this property from Edison.

Recently, the City acquired a piece of property just to the east of 4220 Woodleigh Avenue. It is adjacent to Flint Canyon Trail and it is .12 acres. It was purchased from the County of Los Angeles and was intended to provide scenic beauty along the trail, with possibly a rest stop for equestrians and other trail users.

Access Point Locations.

The following are the existing trail access point locations, as shown on the Trail User Map in Appendix 15:

Hall Beckley Canyon/Earl Canyon Motorway

- South end - Hall Beckley Canyon (City-maintained) – Earl Canyon Motorway (fire road - County-Maintained)
- North end – Mount Lukens Fire Road

Gould Canyon Trail (County-maintained)

- North side of Foothill Blvd., east of Viro Road
- Northwest corner of Flintridge Riding Club (private gated entrance for club members)
- Western side of Jet Propulsion Laboratory (two private gated entrances for JPL employees)
- North end of Viro Road

- East end of Stardust Road (lower)
- East end of Mero Lane
- North end of Alminar Avenue
- Crown Avenue south of Mero Lane
- Middle of Canon de Paraiso
- Gould Avenue north of Knight Way
- Stoneglen where it abuts the trail
- Gould Avenue north of Venado Vista Drive
- East end of Green Lane
- Harter Lane east of Mountain Meadow Lane
- North end of Harter Lane
- Junction with the Cross Town Trail

Horse Lane Trail (County-Maintained)

- North side of Foothill Blvd. west of Daleridge Road
- Baptiste Way east of Crown Avenue
- Junction with Gould Canyon Trail east of Crown Avenue

Cherry Canyon Trails (City-owned and maintained)

- Hampstead Road at Cherry Canyon Motorway
- Sugarloaf Drive (west end)
- Forest Hill Drive (north end)
- Stancrest Frontage Road in Glendale

Cross Town Trail (County-maintained)

- Foothill Blvd at Indiana Ave.
- Flanders Road (west end)
- Alta Canyada Road North of Alta Park Lane
- El Vago St. east of Indian Dr.
- Angeles Crest Highway north of Bay Tree Road

Flint Canyon Trail (County-maintained)

- Hahamongna Watershed Park (in Pasadena)
- Berkshire Avenue at Dover Road
- Woodfield Road
- Chula Senda west of Woodfield Road
- Woodleigh Lane north of Berkshire Ave.
- Oakwood Ave. north of Berkshire Avenue
- Commonwealth Avenue north of Berkshire Avenue
- Beulah Dr. north of Berkshire Avenue
- Berkshire Avenue west of Beulah Drive
- Chevy Chase Drive north of Flintridge Avenue
- Hampstead Road – Cherry Canyon

Staging Area Issues.

A deficiency in the City's trail system is the lack of staging areas. The only existing staging area within the City is the small parking lot off Angeles Crest Highway which provides access to the Cross Town Trail and the Gabrielino Trail. Equestrian trail users now use the Rose Bowl Riders facility or the Equestrian Trails, Inc. (ETI) staging area in Hahamongna Watershed Park or they park along the City's wider streets. One staging area is being planned along the Loop Trail at Foothill Blvd. and Indiana Avenue on property the City intends to license from Edison.

EXERCISE STATIONS

As an additional trail-related amenity, the City may provide and maintain exercise stations. In cases where the property is not City-owned, the City will seek the express permission of the property owner for the exercise stations. Typically, a trail user performs a different set of exercises at each station, such as sit-ups, step-ups or pull-ups, and jogs or walks to the next station. The stations are arranged in a course designed to exercise all of the body's major muscle groups and provide aerobic activity.

CROSSINGS

In the following locations, the trails must cross the 210 Freeway or major arterials:

- Indiana Avenue - Overcrossing the 210 Freeway (planned)
- Verdugo Boulevard near Alta Canyon Rd. (planned)
- Descanso Boulevard near Descanso Gardens (planned)
- Foothill at Indiana Avenue
- Oak Grove Drive and Foothill Blvd. - Undercrossing (tunnel)
- Angeles Crest Highway – North of Bay Tree Road
- Flint Canyon (under Oak Grove Drive and the 210 Freeway)
- Angeles Crest Highway – Undercrossing near Harter Lane

There are numerous other non-arterial, smaller street crossings within the City.

The City should continue to work with the County of Los Angeles, Caltrans and Edison to coordinate appropriate trail crossings at these locations in order to maintain an interconnected and safe trail system.

PROPOSED TRAIL SYSTEM

Since the 1920s, when the trails system was first developed, the trails have been reasonably well protected and enhanced. The vision of a core circular trail was envisioned in the City's General Plan in 1979 shortly following City incorporation. This vision has not yet been realized. Other potential additions to the trail network should also be considered in the future where feasible.

Nevertheless, over the years, the trail network has been somewhat diminished as certain dedicated trail segments have fallen into disuse. Therefore, enhancements to the trails system become even more valuable.

A major objective in trail planning for the future will be to create and preserve a core circular trail within the City of La Cañada Flintridge which connects with other regional and local trails. This can be accomplished by:

The Loop Trail (Phases I, II & III). The purpose of this route is to fulfill the vision of the General Plan to construct the missing link in the trails system, providing a core circular trail. When completed, the final link will connect the trail from north of Foothill Boulevard at Indiana Avenue to the trail at the far southwestern edge of Cherry Canyon. The route, as envisioned in the City's General Plan, can be seen on the General Plan Environmental Resources Master Plan Map (adopted in 1979) in Appendix 9. The core circular trail system, when completed, will provide a trail system which will allow trail users to access the trail system at points on the north, south, east, west and central portions of the City and to completely travel around a large portion of the City (over twelve miles), or choose a lesser distance, and be able to enter or exit the system at multiple locations around much of the perimeter of the City. It will also provide citizens on the westernmost portions of the city with a much more convenient and accessible route to Cherry Canyon and other remote nature areas of the City.

Three segments of the Loop Trail are needed to complete the core circular trail:

- *Loop Trail Phase I.* This phase would begin at the north end of the Caltrans property adjacent to the southernmost end of the Edison property at Foothill at Indiana Avenue. From that point, the trail continues southward across the 210 Freeway using an existing utility flume owned by Caltrans, and continuing westward to the edge of the Caltrans property. Agreements with Caltrans are already in place.
- *Loop Trail Phase II.* This phase would begin on Edison property, near the southwest corner of Indiana Avenue and Foothill Boulevard. From that point, the trail extends southward on Edison property until it reaches the private property that abuts the Caltrans property. From that point, it extends east across an easement granted to the City by that private property owner up to the point where Caltrans property begins (Loop Trail Phase I). The remainder of the Loop Trail Phase II extends from the northernmost portion of the Edison property located just south of the Caltrans property (southern edge of Loop Trail Phase I). Then the trail goes south on Edison property until it meets Verdugo Boulevard. It would cross Verdugo Boulevard

with an in-pavement lighted crosswalk. It would then proceed southward along SCE property, cross the County-owned Winery Channel and continue southward to the edge of Descanso Drive. This portion of the Loop Trail is currently in the final phases of design and environmental review. One portion (from the Loop Trail Phase I south to Verdugo) is scheduled to be completed by March of 2006.

A citizens committee (the Edison Ad Hoc Committee) was formed by Mayor Portantino in 2005 and has completed its recommendations for landscaping and other aesthetic improvements to the Edison property at the corner of Foothill Boulevard and Indiana Avenue. This is in conjunction with the trail development across this parcel. The Committee has presented its recommendation to the City Council and received approval on February 21, 2006.

- *Loop Trail Phase III.* This phase would begin on the north side of Descanso Drive. It would cross south with an in-pavement lighted crosswalk, and connect to the City right-of-way along the south side of Descanso Boulevard. It would proceed west from that point, then turn south at the western portion of the County-owned Descanso Gardens. It would then proceed south along the west boundary of Descanso Gardens until it connected with the fire road adjacent to the southwestern corner of Descanso Gardens.

When implemented, the Loop Trail will finally complete the long-sought core circular trail within the City.

Rockridge Terrace Trail.

A possible trail route exists from Rockridge Terrace east to the YMCA. This trail would traverse the City-owned Rockridge Terrace, and go east toward the YMCA. The development of the full route would only be possible if the property owners and the City are able to enter into cooperative agreements for this trail. However, even if private property easements cannot be obtained, the City plans to develop a trail within the Rockridge Terrace Conservation Area. The development of the Rockridge Terrace Trail would mean more recreational trail use in general for the community, and particularly for those residents living on the west side of the community.

Gateway Link Trail.

In 2006, the City will design the Link District Linear Park. The project is to design a plan to move the traffic lanes to the north on Foothill Blvd. from the commercial district at Leata Lane to the YMCA, thus creating a greenbelt or park on the south side. There will be a meandering multi-modal trail within this greenbelt, extending the entire length of the greenbelt. The intention is to connect the Rockridge Terrace Trail and the Gateway Link Trail on the west in order to create a complete circular loop to and from the YMCA.

San Rafael Hills Trail.

There is a tract map currently being processed by the City (Tentative Tract Map 53647) that is located in the eastern portion of the San Rafael Hills. Access to the project is via proposed extensions and improvements of Inverness Drive/Haverstock Road to the north, Saint Katherine Drive to the east, Palmerstone Drive to the south, and other local streets associated with properties

off Monarch Drive to the west. The project will be approximately 47 acres. In the event this subdivision is approved, a trail should be located in the proposed open space portion of the tract.

Other Possible Trails.

Other potential sites for trails would allow for a greater variety of loops and connections within the trail system.

One possible trail, for example, would connect the Gould Canyon Trail near the Lower Gould Flood Control Basin with the City's existing easement along Angeles Crest Highway which leads to the Gabrielino Trail in Pasadena. Trail users would then have a new in-town loop rather than having to hike up into the mountains to make the loop.

There are also several unused dedicated trail segments in the City. These were dedicated for public use when the neighboring developments were built. The City should consider working with the County to reopen these trail segments in a manner sensitive to the needs of the adjoining landowners.

Future trail planning should also address the need for more neighborhood access points on some trails. The Gould Canyon Trail and the Cross Town Trail in particular have long stretches with limited access. The City should work to develop more access points and to protect existing access legally if necessary. The goal is to bring as many City residents as possible within easy walking distance of the trails.

Similarly, future trail planning should address the need for additional trails, of sufficient length as determined by the City, designed and constructed in compliance with applicable handicap access codes, in order to provide access to the trail experience to as many potential users as possible.

TRAIL SURFACES AND TYPES

Each trail in the City has a different surface, or variety of different surfaces. The type of surface for each trail is shown on the Trails by Surface Map included as Appendix 14. Additionally, Geoweb®, Cellular Confinement System, and other similar products, can be found at some locations on the trails. They are used to control soil stabilization problems, as well as slope, channel, and earth retention problems. The locations of these materials are also noted on the map.

The following are the different surfaces, features with exceptions and types:

Trail Surfaces.

- Dirt/decomposed granite
- Asphalt
- Concrete

Trail Features – Exceptions.

- Crosswalk
- Street

- Driveway
- Bridge

Trail Types.

- Trails
- Fire/Utility Roads
- Proposed Trails
- Streets
- Currently Unused Dedicated Trails

Fire and utility roads are an integral part of the trail network and are indicated as such on the Trails Overview Map in Appendix 12. They connect the trail segments into a continuous whole. They also provide important access to the Angeles National Forest.

TRAIL STANDARDS AND SPECIFICATIONS

Trail standards are necessary to ensure safety, longevity of trails, aesthetics, and recreational opportunities. While it is understood that the standards set forth herein may not be achieved in every instance, they nevertheless represent recommended specifications for trail construction and maintenance.

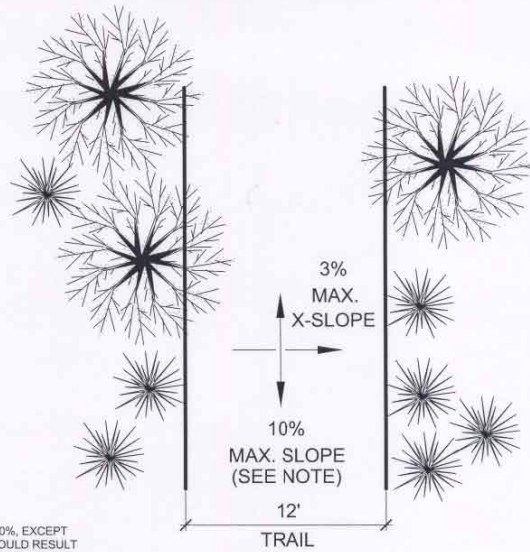
While the City can regulate effects that private property owners in La Cañada Flintridge have on all trails within the City, it cannot regulate improvements or maintenance on non-City owned or maintained trails.

The City can regulate items such as improper drains or drainage on the trail, illegal dumping and obstructions. Therefore, if construction on a trail, such as the building of water bars, etc., is desired, it can only be done by the owner of the trail, or the agency maintaining that trail, within whatever parameters the license, lease or other maintenance agreement may have. This section establishes trail standards and specifications for City-owned trails. To a large extent, the City has used County trail standards in this Plan for consistency, since the County maintains the vast majority of the trails within the boundaries of the City.

The Los Angeles County Standards are as follows:

NOTE:
TRAIL GRADES SHALL NOT EXCEED 10%, EXCEPT
IN AREAS WHERE THIS STANDARD WOULD RESULT
IN EXCESSIVE SWITCHBACKS. IN THIS INSTANCE,
GRADES TO A MAXIMUM OF 15% SHALL BE PERMITTED,
BUT ONLY FOR DISTANCES OF LESS THAN 300 FEET.

TRAILS SHALL BE GRADED SO THE TREAD IS
CROSS-SLOPED ALONG THE ENTIRE LENGTH, AT
A MAXIMUM 3% CROSS-SLOPE.

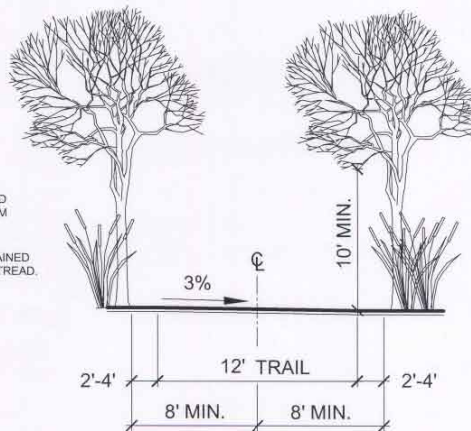


TRAIL GRADES

SCALE: 1/8"=1'-0"

NOTE:
TREES AND SHRUBS SHALL BE MAINTAINED
TO A MINIMUM WIDTH OF EIGHT FEET FROM
CENTERLINE OF THE TRAIL. TEN FEET
CLEARANCE IS RECOMMENDED.

OVERHEAD CLEARANCE SHALL BE MAINTAINED
AT TEN FEET MINIMUM ABOVE THE TRAIL TREAD.
TWELVE FEET IS RECOMMENDED.



TRAIL CLEARANCE

SCALE: 1/8"=1'-0"

LOS ANGELES COUNTY
DEPARTMENT OF
PARKS & RECREATION
PLANNING

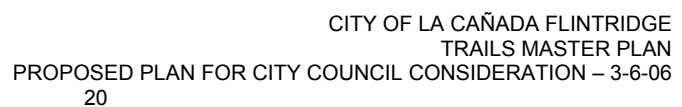
TRAIL GRADES AND CLEARANCE

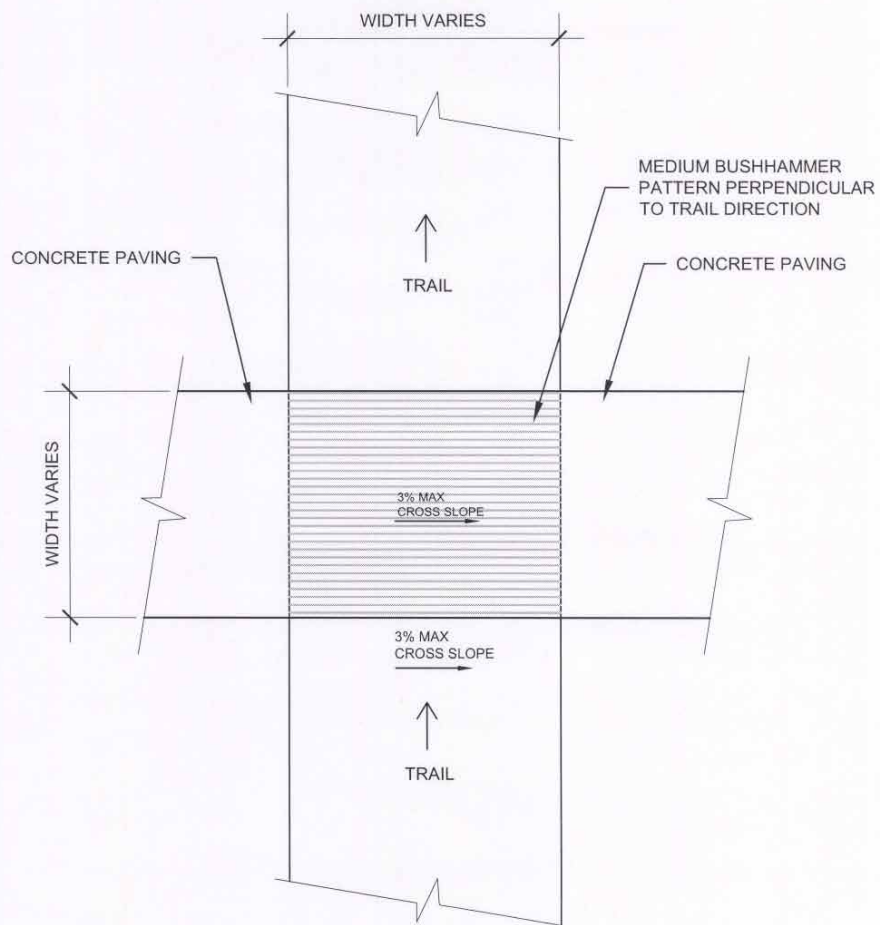
SCALE: 1/8"=1'-0"
DATE: 04-05-05
DRAWN BY: J.B.
REV. BY: S.M.

SHEET

1

OF 1





NOTE: ALL CONCRETE PAVING WITHIN THE TRAIL PATH SHALL BE TREATED WITH A MEDIUM BUSHHAMMER FINISH PERPENDICULAR TO THE PATH OF TRAVEL.

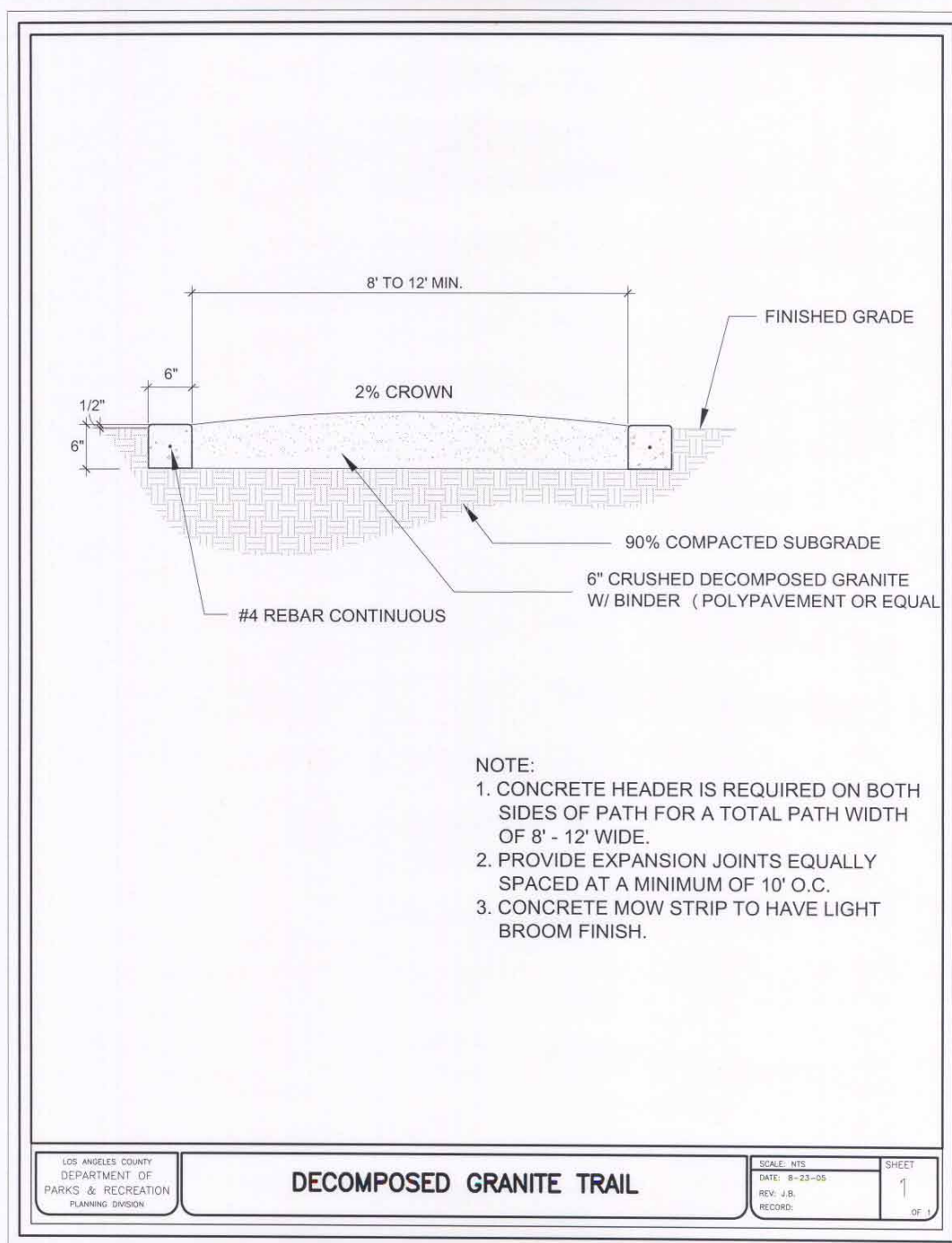
USE A CROSS-HATCH BUSHHAMMER PATTERN WHERE A BIKE TRAIL (HARD PAVED) CROSSES THE SOIL SURFACE TRAIL.

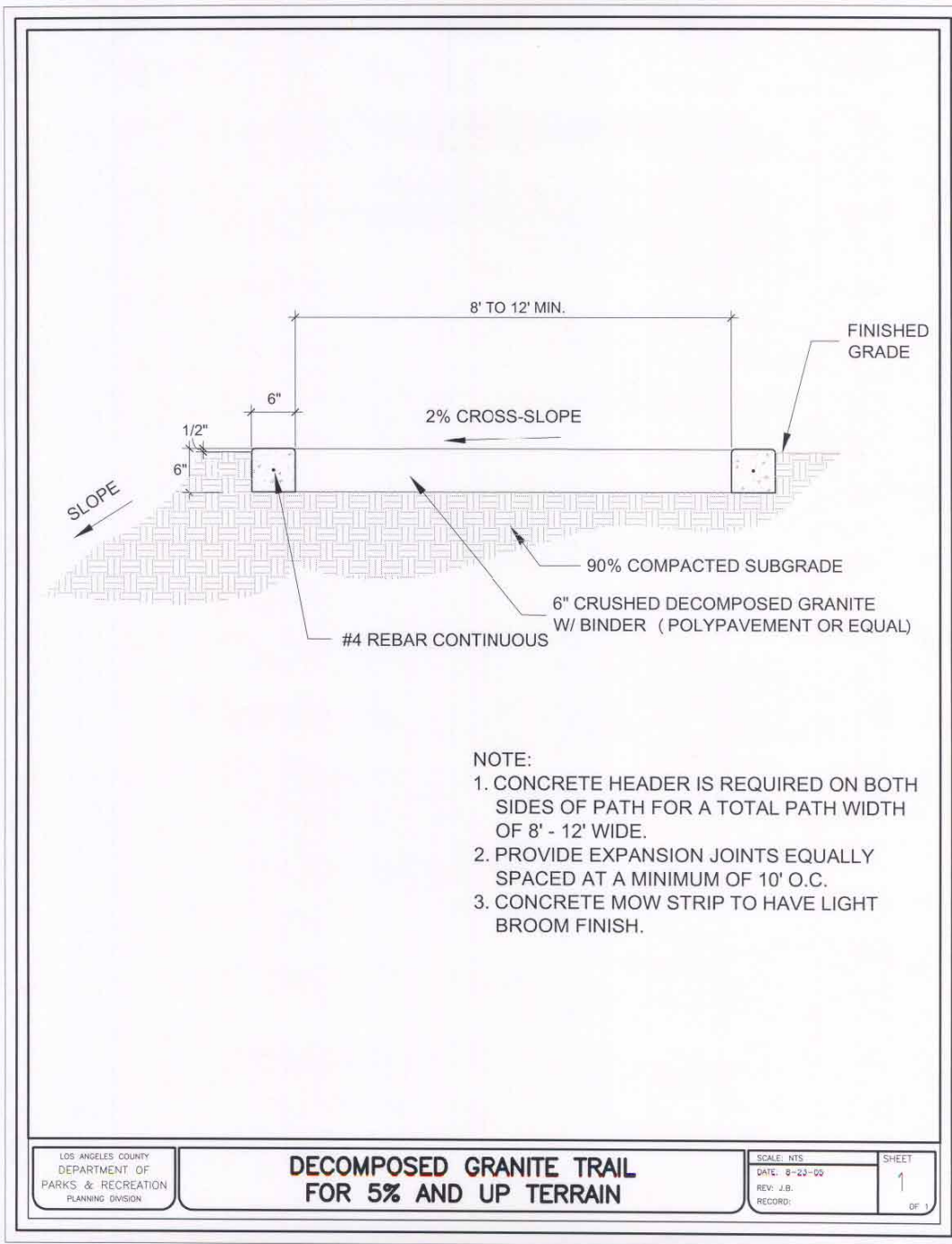
LOS ANGELES COUNTY
DEPARTMENT OF
PARKS & RECREATION
PLANNING

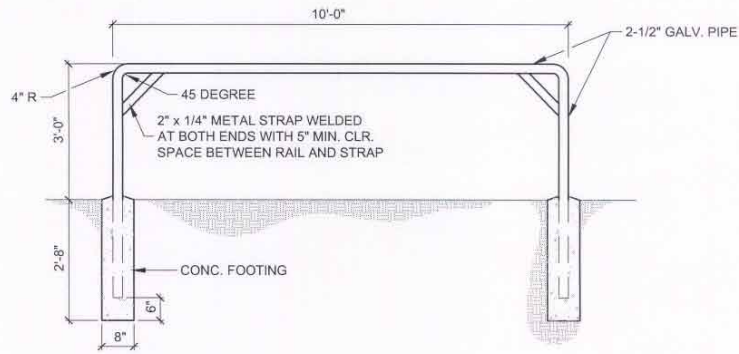
BUSHHAMMER TEXTURE ON CONCRETE

SCALE: NO SCALE
DATE: 04-05-05
DRAWN BY: J.B.
REV. BY: S.M.

SHEET
1
OF 1

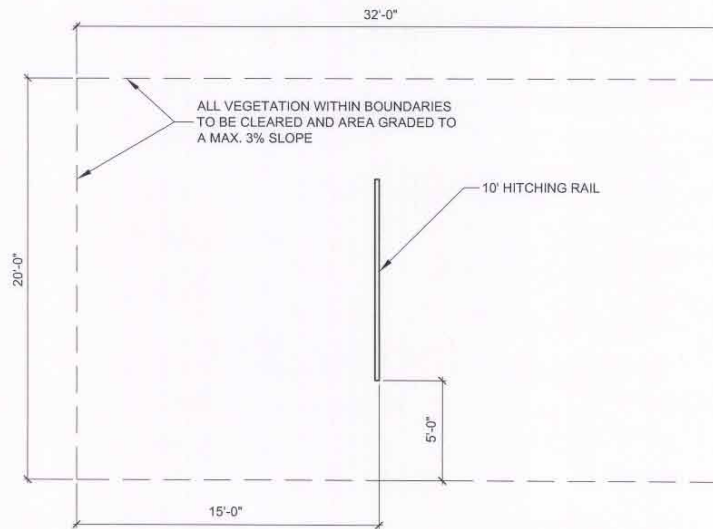






HITCHING RAIL ELEVATION

NO SCALE



HITCHING RAIL PLAN

NO SCALE

LOS ANGELES COUNTY
DEPARTMENT OF
PARKS & RECREATION
PLANNING

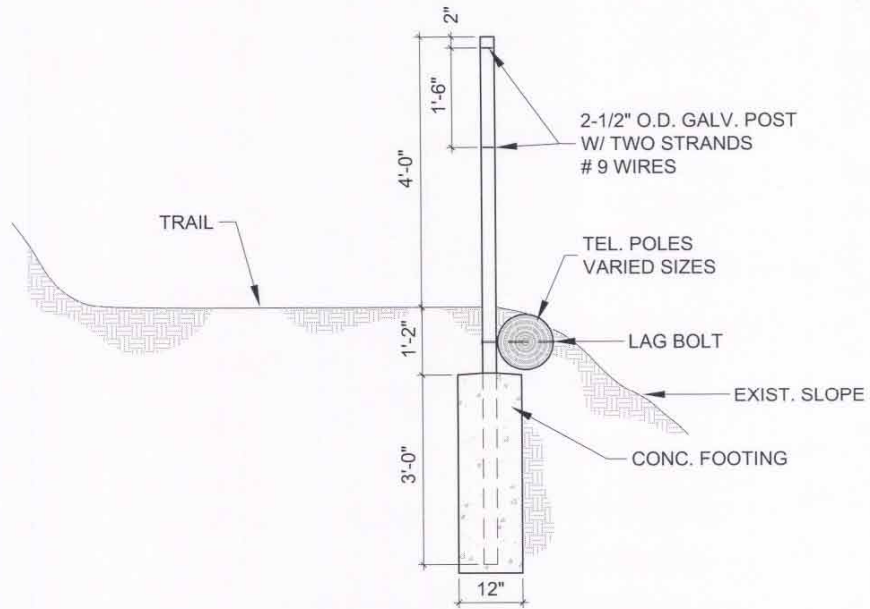
HITCHING RAIL DETAIL

SCALE: NO SCALE
DATE: 11-26-03
DRAWN BY: J.B.
REV. BY:

SHEET

1

OF 1



SAFETY RAIL / SLOPE STABILIZER

SCALE: 1/2"=1'-0"

LOS ANGELES COUNTY
DEPARTMENT OF
PARKS & RECREATION
PLANNING

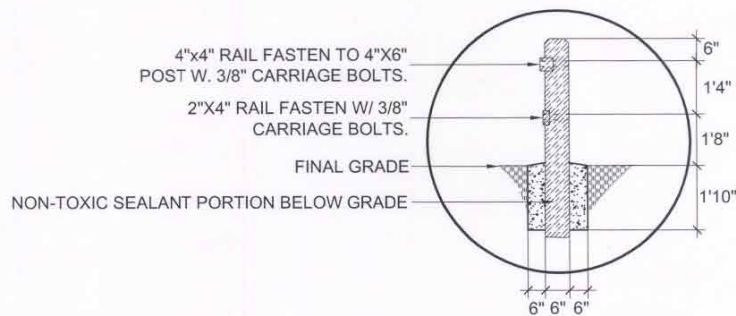
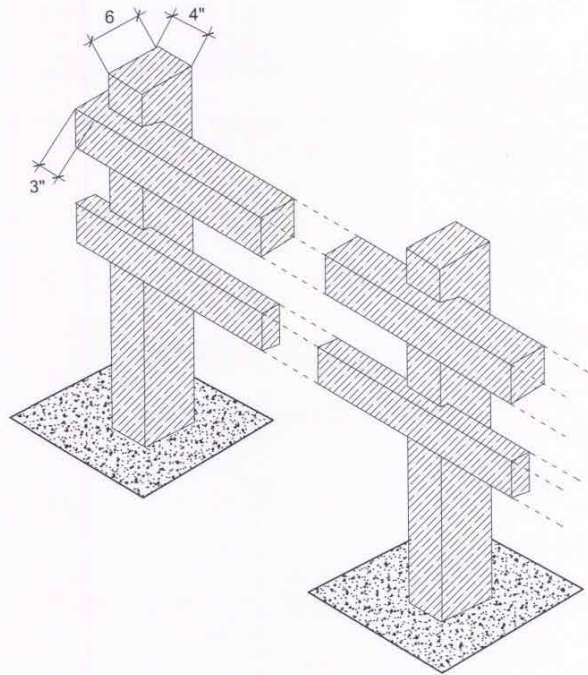
TRAIL-SAFETY RAIL / SLOPE STABILIZER

SCALE: 1/2"=1'-0"
DATE: 11-25-03
DRAWN BY: J.B.
REV. BY:

SHEET

1

OF 1



- NOTE: 1. SPLIT LUMBER SHALL BE USED.
2. POSTS SHALL BE 10'- O.C.
3. STAIN W/ DUNN EDWARDS NO. 15-31 OR EQUAL AS PER MFG. SPECIFICATIONS.
4. ALL METALS TO BE GALVANIZED.
5. NON-TOXIC SEALANT PORTION OF POSTS BELOW GRADE.

LOS ANGELES COUNTY
DEPARTMENT OF
PARKS & RECREATION
PLANNING

SPLIT RAIL FENCING

SCALE: 3/4"=1'-0"
DATE: 11-24-03
DRAWN BY: M.M.
REV. BY:

SHEET
1
OF 1

It is recommended that the foregoing County standards apply to all City and County-owned trails, and in addition that, the following trail standards apply to trails and property under City jurisdiction. The agencies having jurisdiction over trails located within the City but not owned by the City, such as the County of Los Angeles, Edison, and Caltrans, etc., are not subject to these City trail standards, except as provided by agreement with any individual agency.

Construction and Repair Standards.

Effort should be taken to ensure that trails are constructed and repaired to these standards; however, this may not be possible in certain situations due to physical constraints. Variations from these standards may be allowed subject to approval by the Director of Community Development on private property and the Director of Public Works on public property.

The trails are designed to accommodate equestrians, bicyclists, and the full range of pedestrian use including hiking, running, jogging and walking for pleasure. The trail construction and maintenance standards listed below are generally based upon those of Los Angeles County as set forth and illustrated in this section.

Vertical Grade: Long, gradual grades are preferable to short, steep grades; switchbacks may be appropriate and desirable in some conditions.

Acceptable grades are listed below.

- Optimum: 0-5%
- Maximum for distances over 300': 10%
- Maximum for distances limited to 300' or less: 15%

Cross Section Grade: Cross section grades should be kept to a minimum for safe travel. Grades of 2-4% are the optimum condition.

Tread Width: The optimum minimum tread width (actual rideable surface) is 10 feet. This will allow two horses to pass each other with safety and ease. Where necessary, a lesser tread width may be used.

Sight Distances: Adequate sight distances are required at intersections and drive approaches. These distances are to be determined on an individual basis by the City Traffic Engineer. The fence or wall line should end a minimum of 5 feet back from the top of the "X" at drive approaches. *Note: A typical drive approach consists of a flat portion where the vehicles drive over. On either side of the flat portion are the "Xs". The width of the "Xs" depends on the height of the curb face. The "X" is the transition from 0" curb face to full height curb face. The "bottom of X" is at 0" curb face and the "top of X" is at full height curb face of the transition.*

Clearance: Vegetation should be preserved as much as possible to preserve the rural character of the trails and to prevent erosion. Trees and shrubs should be maintained to a minimum width of 8 feet from the centerline of the trail. Ten feet clearance is recommended. Overhead clearance should be maintained at ten feet minimum above the trail tread.

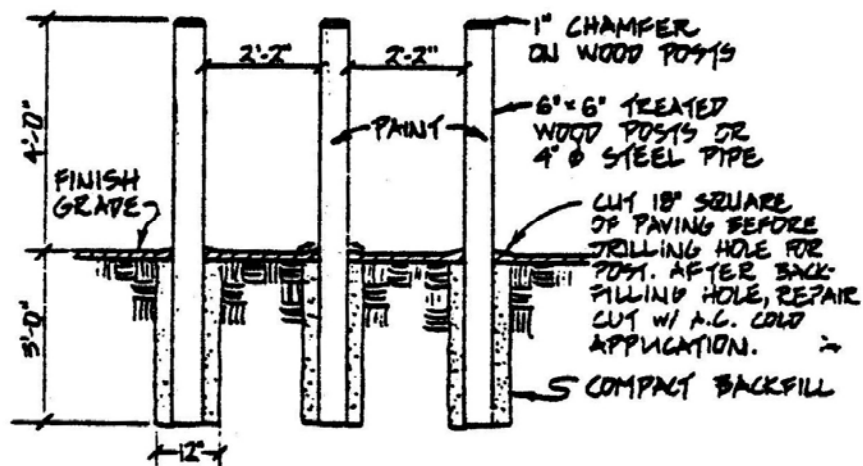
Drainage: Drainage is a very important consideration in trail construction and maintenance. The method used to drain the trail will depend largely on the location of the drains, the velocity of water and the type of soils in the area. Where a trail gradient exceeds 4%, water bars, splash curbs, or other diversionary devices should be required.

The trails have often been used to drain neighboring back yards, a practice which has caused numerous erosion problems. Adjoining properties should not be permitted to concentrate water flows onto trails. Any drainage onto the trails should be mitigated by appropriate devices as specified and approved in writing by either the Community Development or Public Works Department so as to prevent erosion damage and standing water. Landscaping and irrigation plans for properties along the trails should be approved only if they cause no negative impact upon the trails. All irrigation systems should be designed and implemented to eliminate any direct spray or water reaching the trail surface. Drip irrigation is encouraged as is the use of drought-tolerant plants so as to prevent water drainage onto the trails. No private property irrigation construction on trails will be allowed. Drainage plans are to be approved by the Director of Public Works and/or the Director of Community Development or their designees.

Surfacing: Surface should be decomposed granite, where possible, with a 3" minimum base. Subsurface preparation should include removal of rocks and debris and grading the surface smooth. Retaining walls, safety rails and slope stabilizers shall be used as necessary along the sides of the trails.

Concrete paving within the trail path should be treated with a medium bushhammer finish perpendicular to the path of travel. In the few areas of the City where trail users must travel on the street, textured pavement, such as open graded asphalt, should be used to prevent horses from slipping.

Barriers: In addition to signing, it may be appropriate in certain situations to use barriers to prevent unwanted trail usage by motorized vehicles. Where feasible, barricades should be recessed 15' back from the street entrance. A variety of barriers may be used for this purpose, such as bollards (or telephone poles laid horizontally) may be used. Bollards should be installed as shown below:



BOLLARD PLACEMENT

Street Crossings: For safety reasons, both trail users and motorists must be warned of crossings. Signs and pavement markings should be used and augmented by flashing signals, etc., as warranted by the traffic flow and as indicated by the Traffic Engineer. The City should paint all curbs in front of trails at crossings red, wherever appropriate, to prevent parking that blocks access to trails and promote safety for trail users.

Fences and walls: Most of the trails that run through the City are bordered by fences and walls which demarcate the trail boundaries and provide privacy for the adjoining homes. Suitable materials for fences and walls include wood, concrete block or chain link in combination with hedges, ivy and other plantings. Netting, cloth coverings, fabric or other fence coverings are not acceptable. Barbed wire fencing is not permitted along the trails. Any fencing that could potentially injure trail users or their horses shall not be allowed. Sharp wrought iron fencing is not safe along the trails. It is recommended that the City Zoning Code be amended to exclude such materials and/or structures. No fences or walls shall encroach upon trail easements without approval from the City. Fencing along trails shall be subject to approval of the City.

Maintenance Standards.

Trails, bridges, fencing, vehicle barriers, lighting, and signs should be inspected at regular intervals for safety hazards, damage, or other needed maintenance or repairs.

Proper grade and surfacing of all trails should be continuously maintained, including, but not limited to, removal of loose rock in excess of 4-6" diameter or larger, filling holes, and refilling with new surfacing material to required depth, as needed.

Vegetation should be kept cleared to a height of ten feet, to the minimum trail width, and to maintain adequate sight distances on street crossings as determined by the City Traffic Engineer on a case-by-case basis.

Poison oak should be removed at regular intervals.

Weeds should be regularly abated to keep trail surface free of weeds.

Use of chemicals should be avoided, whenever possible, because of environmental concerns as well as potential ingestion by horses. If such use is necessary, a sign describing the chemicals used should be posted on the trail.

Stolen, damaged, or obsolete signs should be routinely replaced.

Trailhead/Trail Entryway Demarcation.

Prior to the City constructing trailhead/trail entryway demarcation markings, the City should design a prototype for these entryways in keeping with the Master Plan. A formal, but simple structure, with distinctive markings, but varying amenities, should accompany each entrance into the trail network. The prototype should be something similar to the following:



Signs.

A well-maintained network of signs is important to the trail system so that users can easily find their way. The existing trail signs provided by the La Cañada Flintridge Trails Council will continue to be used at major trailheads. The following is a photo of an existing Trails Council sign:

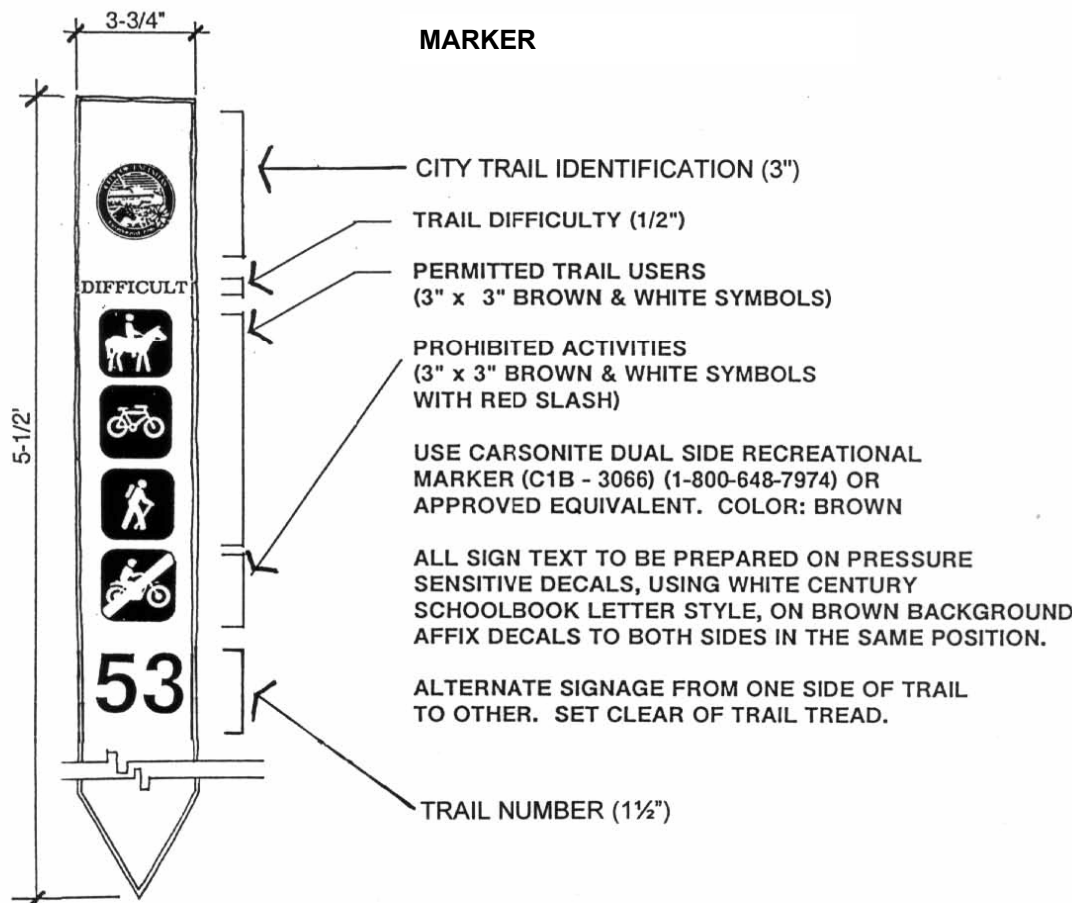


The Trails Council signs will be augmented by trail markers. These will be rectangular signs, 4 feet high by 6 inches wide attached to the same pole similar to the illustration below.

Additional information such as permitted trail users, precedence of trail uses, and other regulatory information may be included.

At other locations where signs may be necessary, a smaller dual sided flexible marker shall be used as shown below which will include similar information to that of the larger signs but will be less intrusive to the rustic nature of the trail. The information included on both types of signs may vary according to location.

NOTE: DETAIL SHOWS TYPICAL SIGNAGE.
CONSULT CITY FOR SPECIFIC TRAIL NUMBER, DIFFICULTY, & PERMITTED
TRAIL USERS FOR EACH TRAIL SEGMENT.



In a few locations where there may be confusion because of side trails or the like, a small directional sign, 1 foot by 1.5 feet, with the word "TRAIL" and an arrow below will point trail users in the right direction.

Trail Decorative Elements.

The trails in La Cañada Flintridge, although they contribute greatly to the semi-rural charm of our town, have received very little embellishment. With the exception of an arched entryway on Horse Lane built as an Eagle Scout project in 1988, there are few decorative elements along the

trails. As a result, this valuable community asset is neither emphasized nor celebrated in our streetscape. New trail users often stumble upon the trails by accident.

Simple split rail fencing made of wood, concrete, vinyl, or fiberglass should be placed on one side of trail entryways at locations chosen by City staff in consultation with the La Cañada Flintridge Trails Council. Native plantings should also be considered where feasible to increase the attractiveness of trail entry points.

TRAIL USERS

Trails in La Cañada Flintridge have long been considered shared trails on the part of many groups, including equestrians, walkers, hikers, families, bicyclists, commuters and other non-motorized users. The trails have been used cooperatively, without the need for divided trails for different users. Trail users must yield on the trails as follows: bicyclists yield to pedestrians and equestrians, and pedestrians yield to equestrians.

No motorized vehicles except wheelchairs, authorized maintenance vehicles, as well as patrol, emergency and utility vehicles will be allowed on trails. If conflicts arise on specific trail segments, use may be restricted to avoid environmental degradation or to protect users from dangerous trail user conflicts.

It should be noted, however, that the driving force behind the establishment of La Cañada Flintridge's trails has primarily been the equestrian community. There has been a long tradition of equestrian life that is a unique part of La Cañada Flintridge's history and heritage. There are approximately 200 horses in the City, including those boarded at Flintridge Riding Club.

Members of the following organizations are regular user groups of the City's trails:

- Rose Bowl Riders, Inc. - P.O. Box 226, La Cañada Flintridge, CA 91012
- Corral 2 of Equestrian Trails, Inc.
- La Cañada Oaks Mountain Biking Association - P.O. Box 1350, La Cañada Flintridge, CA 91011
- Pasadena Chapter – Sierra Club - P.O. Box 94086, Pasadena, CA 91109-4086
- Angeles Chapter – Sierra Club – 3435 Wilshire Boulevard, Suite 320, Los Angeles, CA 90010-1904
- La Cañada Flintridge Trails Council, P.O. Box 852, La Cañada Flintridge, CA 91012
- Roger Barkley Community Center – 4469 Chevy Chase Drive, La Cañada Flintridge, CA 91011
- Flintridge Riding Club – 4625 Oak Grove Drive, La Cañada Flintridge, CA 91011-3758
- Altadena Crest Trails Restoration Working Group
- Mach 1 (Move a Child Higher, Inc.) - 1430 Topeka Street Pasadena, CA 91104
- Equestrian Trails, Inc. - Corral 2 - PO Box 361, Pasadena, CA 91102
- Tom Sawyer Camps - 707 West Woodbury Rd, #F, Altadena, CA 91001
- La Cañada Unified School District Schools, as well as private schools and their students

PUBLIC EDUCATION

The City should conduct education about trail usage, standards, safety, allowable or preferred landscape and irrigation installation practices and respecting the rights of both the private property owner and the trail users. The City should sponsor activities on the trails promoting use and educating citizens about the nature, extent and usefulness of the City's trail system. The City should produce and distribute a brochure regarding private property owner responsibilities regarding trails. The brochure should also be distributed to contractors, landscapers, real estate companies, and architects with projects or clients on a trail within the City.

MANAGEMENT

Good trail management is key to enhancing the image of trails in the community and promoting their use. The City shall manage its own trails and inform the County of pertinent trail management and maintenance issues. Appendix 16 is the Trails by Maintenance Map that shows both City-maintained and County-maintained trails.

The following guidelines provide an effective framework for trail management:

Management of Trail Encroachment Issues. Encroachment onto City owned and/or City maintained or County owned and/or maintained trails or onto a currently used trail on private property should be reported immediately to the Community Development Department. If the trail encroachment is on public property, or if most of the trail encroachment appears to be on public property, the encroachment should also be reported to the Public Works Department. However, where the issue stems from private property owner use on public property, the incident should be handled by the Community Development Department.

In cases where the trail is on property not owned by the City, but owned by the County, Edison, or Caltrans, the encroachment should be reported to the agency owning the trail. In the event the encroachment is by a private property owner, the City should enforce all applicable City ordinances in order to remove the encroachment. The City should then work in cooperation with the agency owning the property in an effort to eliminate the encroachment. In the event an obstruction or partial obstruction has been constructed by the non-City agency owning the trail, the City should work cooperatively with that agency in an effort to remove or relocate the obstruction on the trail.

The City should investigate the encroachment and make every attempt to resolve the issue informally. In the event the encroachment is confirmed, and where the encroachment is part of construction activities requiring building permits, the City should issue a Stop Work Order and inform the contractor and the property owner. Issues not resolved in a timely manner may be referred to the City Prosecutor for resolution.

Management of Private Property Owner Rights. The Trails Master Plan is intended to respect both the rights of the trail user, as well as the rights of the private property owner adjacent to the trail. Private property owners with issues regarding trail use should contact the Community

Development Department. The City should make an effort to work with trail users and property owners to find solutions in cases of disputes. When it is appropriate and possible, the City should lend its assistance, including the use of the City Prosecutor regarding compliance issues.

Emergency Response and Mitigation. The City should include the trails in its emergency response plan, as well as in the Hazard Mitigation Plan for the trails.

Inform public in advance, when possible, of trail closures. The City may close any trail for maintenance or safety purposes. When possible, the public should be notified of trail closures at trailheads and at the points of closure. Notification should include dates and reasons for closure. The County may also close its trails. Edison has the right to close the trails on its properties or easements at any time (without notice) for any utility maintenance and/or safety reason. Where possible, Edison will make every effort to notify the City and/or County when such closures occur.

METHODS OF ACQUISITION

The City has several means of acquiring both land in fee or easements for trails use. These include dedications, leases, and/or donations, cooperative agreements, and purchases. In cases where the city is seeking a grant to develop the trail, in general, the land tenure provisions required as a condition of the grant are long-term (fifteen to twenty years). Therefore, the trail rights obtained on property not owned by the City must be long-term to obtain grant funding. In cases where the easement or other right granted cannot be long-term and the trail must be relocatable, most often those funds would have to be general fund monies.

Dedications, Leases, and Donations of Property.

Acquisitions from Developers:

Governing law permits the City to require a dedication of land (for example, a trail segment) as a condition of a land use approval (for example, a hillside development permit or a subdivision map approval). This Plan supports such dedications, keeping in mind the following legal requirements: (1) the City has the burden of proving a sufficient nexus exists between the required dedication and the impact of the proposed development (i.e., the use of the trail segment at issue by the future inhabitants of the area to be built out); and (2) No precise mathematical calculation is necessary to show the required reasonable relationship, but the City must make some sort of individualized determination that the required dedication is related, both in nature and extent, to the impact of the proposed development--that it is roughly proportional.

Acquisition from Public Agencies:

When the sale of public property occurs within the City, the City should examine the property for potential trail usage and consider acquisition where appropriate.

Dedication of Trails Easement by Property Owner:

Dedication of easements may be obtained from property owners if they wish to grant them without remuneration. Donations of trail easements for public access may qualify as an income tax-deductible charitable gift as long as the easement is perpetual.

Land Leases:

Some landowners may opt to consider a land lease. A lease can be seen as a “trial period” before the property owner commits to a permanent arrangement. While land leases offer maximum flexibility to the landowner, they are undesirable to the City because of the lease’s temporary status.

Donations of Property:

The City may accept donations of property at any time for trails when in the City’s best interest to do so. Land donations may generate tax benefits that equal or outweigh the unrealized gain of a conventional market-rate sale. The property owner should be informed that this alternative may exist and urged to consult a personal tax advisor.

Cooperative Agreements.

The City should formulate joint agreements with other public agencies, such as Los Angeles County, Caltrans and certain water purveyors to establish trail segments. Caltrans requires a maintenance agreement and in some cases, a Memorandum of Understanding

In addition, the City should formulate such agreements with private agencies which are responsible for and/or owners of utility corridors, such as Edison, to establish trail segments, with the understanding that such agencies are generally precluded from considering such segments as permanent due to operating needs and requirements which may change over time. Edison requires a license agreement for its fee-owned properties, which is provided for a specified term and which can be renewed at the end of the license period; however, all Edison licenses include a 30-day cancellation clause, which can be invoked by the licensee (City or County) or Edison under specified conditions. In addition, where Edison may have easements, it reserves the right to review and comment on proposed trail uses there.

The City, in conjunction with the County of Los Angeles, could also enact an ordinance in accordance with Government Code Section 51200 et seq., the California Land Conservation Act of 1965, which would permit the City to acquire trail easements from landowners in return for lower tax assessments on that portion of the property.

Purchase.

Although purchase of easements or land in fee is the least preferred alternative, due to cost, it may prove necessary on private parcels. The City supports the integrity of the trail network and will consider all legal means, as necessary and appropriate, to keep the system intact should any trail segments be threatened with closure or where public access to the trails is denied or restricted.

A land owner may choose to sell the land as a bargain sale transaction. This is part donation and part sale to a government agency or a qualified non-profit organization. A bargain sale may entitle property owners to an income tax deduction for a charitable contribution and reduction in capital gains tax.

METHODS OF FUNDING

The following are ideas for funding through Local and State Federal sources:

Local Funding Sources.

Developer Fees -The California Planning, Zoning and Development Laws (California Government Code Section 66477 - Quimby Act) authorizes cities and counties to require the dedication of parks and recreational land or the payment of in-lieu fees as a condition of tentative subdivision map approval, under appropriate circumstances. Since most land in the City of La Cañada Flintridge that is developable is already developed, the opportunities to collect developer fees are limited.

Bonds and Assessments - In the past, various State and local agencies have successfully used bonds or assessment districts to fund improvement projects or acquisitions.

Fund Raising - Special Events - The City, with the assistance of trail advocacy groups like the La Cañada Flintridge Trails Council, and other user groups, could sponsor a special event such as a trail ride or walk-a-thon. This could take place along a portion of the trail that has safe passage, but is in need of improvement or along completed segments adjacent to segments that need acquisition or development. Events of this kind help to generate revenues as well as build support for the further development and continued maintenance of the trail system.

Grant Funding Sources (State or Federal).

- AB 2766 Vehicle Registration Funds – These funds are also referred to as AQMD Funds and can be used generally for bike lanes, bike lane improvements and pedestrian amenities such as crosswalks, shelters and bike racks.
- Bicycle Transportation Account Program – Funds projects that improve safety and convenience for bicycle commuters.
- California Wildlife Coastal and Parkland Conservation Act of 1988 - Competitive grants are available for acquiring and developing land and rights-of-way for bicycle riding, horseback riding, hiking and handicapped access trails. In addition to the cities and counties, non-profit organizations like Solano County Farmlands and Open Space Foundation are eligible to receive grant money.
- Environmental Enhancement and Mitigation Program (AB 471) - This program is administered by the California State Resources Agency and is available for projects that provide mitigation for the environmental effects of transportation facilities.
- Federal Highway Administration Funds - These funds are made available based on a three year Transportation Improvement Program (TIP) prepared by the Los Angeles County Metropolitan Transportation Authority (LACMTA)
- Habitat Conservation Fund - The Habitat Conservation Fund is available to open space and recreation-oriented trail projects. Competitive applicants are primarily large scale projects that link natural corridors. No on-street trails are funded by the Habitat Conservation Program. This program is administered by the State Department of Parks and Recreation, and requires matching funding.
- Land and Water Conservation Fund (CA Dept. of Parks and Recreation) – Funds non-motorized trails.

- Roberti-Z’Berg-Harris Urban or Block Grant Program - This program provides funds for acquisition and development or rehabilitation of park lands and facilities and also provides safe, open and accessible park and recreational opportunities.
- Safe Routes to School Program (SR2S; Caltrans) – This source provides funds for pedestrian and bicycle safety and traffic calming projects that provide safe routes to school for students. It includes on-street bicycle facilities and off-street bicycle/pedestrian facilities.
- Safe, Accountable, Flexible and Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) – 2005-2009 - The U.S. Department of Transportation's Federal Highway Administration is the nation's largest single source of funding for multiple use trails and related projects. The new transportation law (SAFETEA-LU) includes significant funding for trails through programs including Transportation Enhancements and the Recreational Trails Program - that fund trails.
 - *Transportation Enhancements (TE)*: The TE program funds a wide variety of transportation-related community projects, but trails and pedestrian/bicycle facilities have historically accounted for about half of TE funding since its establishment in 1991.
 - *Recreational Trails Program (RTP)*: Provides funds for maintenance and restoration of existing recreational trails; purchase and lease of recreational trail construction and maintenance equipment (motorized projects only). Provides development and rehabilitation of trailside and trailhead facilities and trail linkages for recreational trails; construction of new recreational trails; acquisition of easements and fee simple title to property for recreational trails or recreational trail corridors (non-motorized projects).
 - *Nonmotorized Transportation Pilot Program*: This program is to help develop trails and other pedestrian/bicycle facilities.
 - *Congestion Mitigation Air Quality Program (CMAQ)*: Helps communities with air quality problems invest in less-polluting alternative forms of transportation, including pedestrian/bicycling facilities and trails.
- Santa Monica Mountains Conservancy Grants – Funds for trails are available through this source, and the City has previously been a recipient of these funds for the Ultimate Destination Trail, the Flint Canyon Trail, the Conservancy Trail, and the Hall-Beckley Conservancy Area.
- State Transportation Improvement Program (STIP) – Funds for pedestrian and bike facilities are available through the STIP. However, currently most STIP funds are suspended due to State budget problems. Allocation of new projects will depend on the fiscal condition of the State.
- Surface Transportation Program (STP) – This source provides funds for bicycle and pedestrian facilities.
- Transportation Development Act (TDA) – Article 3 - TDA Article 3 funds are available for bicycle and pedestrian facilities.
- Urban Stream Restoration Program - This program provides grants to assist local agencies and organizations with on-site restoration work including erosion control, landscaping, and creek channel maintenance.

- State Urban Parks and Healthy Communities – Provides funds for recreation activities that involve physical exertion and occur within a facility or venue that was specifically designed for this pursuit.
- Community Based Transportation Planning Demonstration Grant Program – This is a Caltrans Program that supports demonstration planning studies (not capital improvements).
- Hazard Elimination Safety Program – This is a federal safety program that provides funds for safety improvements on all public roads and highways. These funds serve to eliminate or reduce the number and/or severity of traffic accidents at locations selected for improvement. Such funds can be used for pedestrian crossings.

Unfortunately, the availability of federal, state, regional, and locally sponsored program funding remains low. This scarcity of money has created a highly competitive atmosphere for obtaining funding from state and federal sources.

Other Sources of Funding and Labor.

Cost savings can be achieved by using alternative sources of labor such as state subsidized labor, private landowner assistance, and citizen involvement.

California Conservation Corps (CCC) - The CCC offers an opportunity for State-funded implementation of a trail system. CCC field crews are capable of providing trail construction and maintenance. Members of the crew have moderate and specialized technical skills levels. Los Angeles California Conservation Corps (LACCC) crews were successfully in La Cañada Flintridge for the building of the Conservancy Trail in 2005.

Citizen involvement. Active citizen volunteers can help construct and maintain trails.

LIABILITY

Trails Volunteers. The City of La Cañada Flintridge encourages volunteerism and provides worker's compensation coverage for trails volunteers as long as they are in compliance with Resolution No. 04-50 authorizing the City Manager to determine the provision of workers' compensation coverage for certain City volunteers pursuant to the provisions of Section 3363.5 of the California Labor Code (see Appendix 10 for full text). When trails volunteers request coverage, the City should check the Geographic Information Systems trails map to determine which agency maintains the trail. Additionally, the City should consult the Trails Council to advise the City regarding the viability of the trails project.

Private Property Owners. Private property owners who have formally granted access to trail users via an easement are not exposed to increased liability by those trail users who are injured while using a trail on their property or trespassing onto their property from a nearby trail, subject to the provisions of law. State law provides certain protections to private property owners against liability in such cases (e.g. California Government Code Section 830-831.9, California Civil Code section 846). See Appendix 10 for full text.

Edison. Wherever Edison licenses property on its fee-owned property, it requires the licensee (the City or the county, depending on the trail) to provide proof of insurance in accordance with relevant license agreements.

MAINTENANCE PLAN

Maintenance.

- *City Owned and/or Maintained Trails.* Trail maintenance and repair problems should be reported to the City Public Works Department, which is responsible for the maintenance of City-owned trails, as well as City-maintained trails on Caltrans and Edison property. The City has a regular maintenance schedule, including brush clearing and poison oak treatments, as follows:
 - Trails are cleared four times per year, five feet on either side of each trail.
- *County Owned and/or Maintained Trails* – County trail maintenance and repair issues should be reported directly to the County Trails Maintenance crews, or through the City Public Works Department, which will then report them to the County. The County is responsible for the maintenance and repair of the County-owned trails and on property where the County has easements or other rights for trails. It is also responsible for County-maintained trails on Edison property.

The City, at the request of a resident or of the County, where appropriate, on any trail in the City, will conduct enforcement activities regarding problems on the trail such as water damage from sprinklers, illegal drainage, debris dumped on the trails, or other such maintenance issues. In the event a trail is damaged by these activities or by construction, the City will require the private property owner responsible to restore the trail.

Whenever a street is resurfaced or reconstructed, all trail crosswalks and signs should be immediately replaced.

Nothing in this section will prohibit Edison, or contractors acting on behalf of Edison, from having access to the trails for purposes of operation and maintenance of its electrical system, particularly where the trails are located on property licensed to the City or County by Edison.

Management Issues.

- *Maintenance and Facilities Coordinator.* -The City currently has a Maintenance and Facilities Coordinator position assigned to oversee trail maintenance activities, which include response to resident questions, periodic checks of the trail system, an evaluation of the trails where necessary, and oversight of contract maintenance personnel. Where corrective action is needed, contract personnel are dispatched to perform the necessary work including clearing of vegetation, weed control, repair of drainage or erosion problems, and replacement of stolen, damaged, or obsolete signs. The La Cañada Flintridge Trails Council also donates trail signs. If the need arises, the City should consider creating the part-time position of Trails Coordinator so that all trails issues are coordinated through one employee.
- *Recreational Trail Inspection Report Form.* - The City should use a Recreational Trail Inspection Report Form (Appendix 11) to document the conditions of trail segments. This is a critical

link in the process of accident prevention. It is a practical way to prevent foreseeable accidents because it prompts employees and supervisors to systematically inspect for hazardous conditions and remedy these conditions before they exist. This should be posted on the City's website for use by residents.

- *Encroachments.* - All parties that have encroached on the designated trail should be immediately notified that removal of these obstructions is required. Encroachments include, but are not limited to, fences, walls, irrigation components, drainage pipes, decorative structures, storage of materials, construction and yard debris, shrubs, and trees and planters. The encroachments may pose a safety and liability risk to the City and to the encroacher; therefore, their immediate correction is recommended. The City may use the City Prosecutor, the Planning Commission and the Public Works and Traffic Commission where necessary to resolve the issue.
- *Motorized Vehicles Control.* - Except for authorized maintenance, emergency and utility vehicle access needs, the trail system is for non-motorized use only. Any size electrical vehicles are also not allowed. Trail entry points will be designed to discourage motorized vehicle use. Trail signage, barriers, fences, and active trail management will help restrict use by motorized vehicles. Trails may not be used for storage of construction equipment. Ingress and egress to private property by construction vehicles during the construction process must be approved by the City as a part of the permit process.
- *Trail Closures.* - Some trail sections may need to be temporarily closed due to hazardous conditions. These may occur when heavy rains cause trail washouts or when landslides block the use of the trail. Construction on or adjacent to the trail could also force the closure of the trail because of safety concerns. These conditions should be corrected as soon as possible and the trail reopened to use by the public. Any unsatisfactory condition and corrective action could be noted on the Recreational Trail Inspection form.

When a trail must be closed due to construction, the City should advise the individuals performing the construction that the area must be posted prior to the closure with notice of the dates that the trail will be closed and advising of an alternative route. The signage should also indicate the expected date of trail re-opening. The City should work with the applicant to design an alternative route to ensure safety and convenience to trail users, wherever possible. Closure of the trail should be minimized. Prior to reopening, City staff should inspect the trail to ensure that the trail is safe and ready for use.

- *Volunteer Management.* - In many cities, volunteer organizations not only advocate for trail implementation, but also physically maintain large portions of the trail system. Groups such as this are critical to the full development of the trail system and its continued maintenance. The City should also explore "trail adoption" programs, inviting service organizations to undertake patrol and maintenance of other segments. The La Cañada Flintridge Trails Council, with assistance from the City, can continue to organize a "Trail Maintenance Day" where large numbers of citizens could be called into assistance for a periodic clean up of the trail system. Also, a "Trail Promotion Day" could be organized utilizing volunteers.

BACKGROUND OF THE TRAILS MASTER PLAN

The City Trails Ad Hoc Committee was appointed in 2002 by Mayor Anthony J. Portantino. The Mayor charged the Committee with constructing a “comprehensive document that identifies all of our existing [trail] resources, plans for future development and connectivity, and puts in place a blueprint for the City Council, staff and Trails Council to follow for continued enhancement and protection of these valuable recreational assets.”

The resident members of the Committee are:

- Mary Barrie – Committee Chair; Parks & Recreation Commissioner; and La Cañada Flintridge Trails Council President
 - Gregory C. Brown - Committee Vice-Chair; and Mayor Pro Tem
 - John Connelly - Committee Member; and Public Works and Traffic Commissioner
 - Jon Moldafsky – Committee Member, Parks & Recreation Commission Chair
 - Randy Strapazon – Committee Member, Member of the La Cañada Flintridge Trails Council
- Steve Chuck also served as a Member of the Committee, as well as being a Member of the La Cañada Flintridge Trails Council, and resigned in 2005.

The outside agency members of the Committee are:

- Christine McLeod, Southern California Edison
- David Law, Southern California Edison

Nolan Lam, Southern California Edison, also served until he moved on to other duties within SCE. Tonda Lay, Los Angeles County, was a member of the Committee, and attended several meetings.

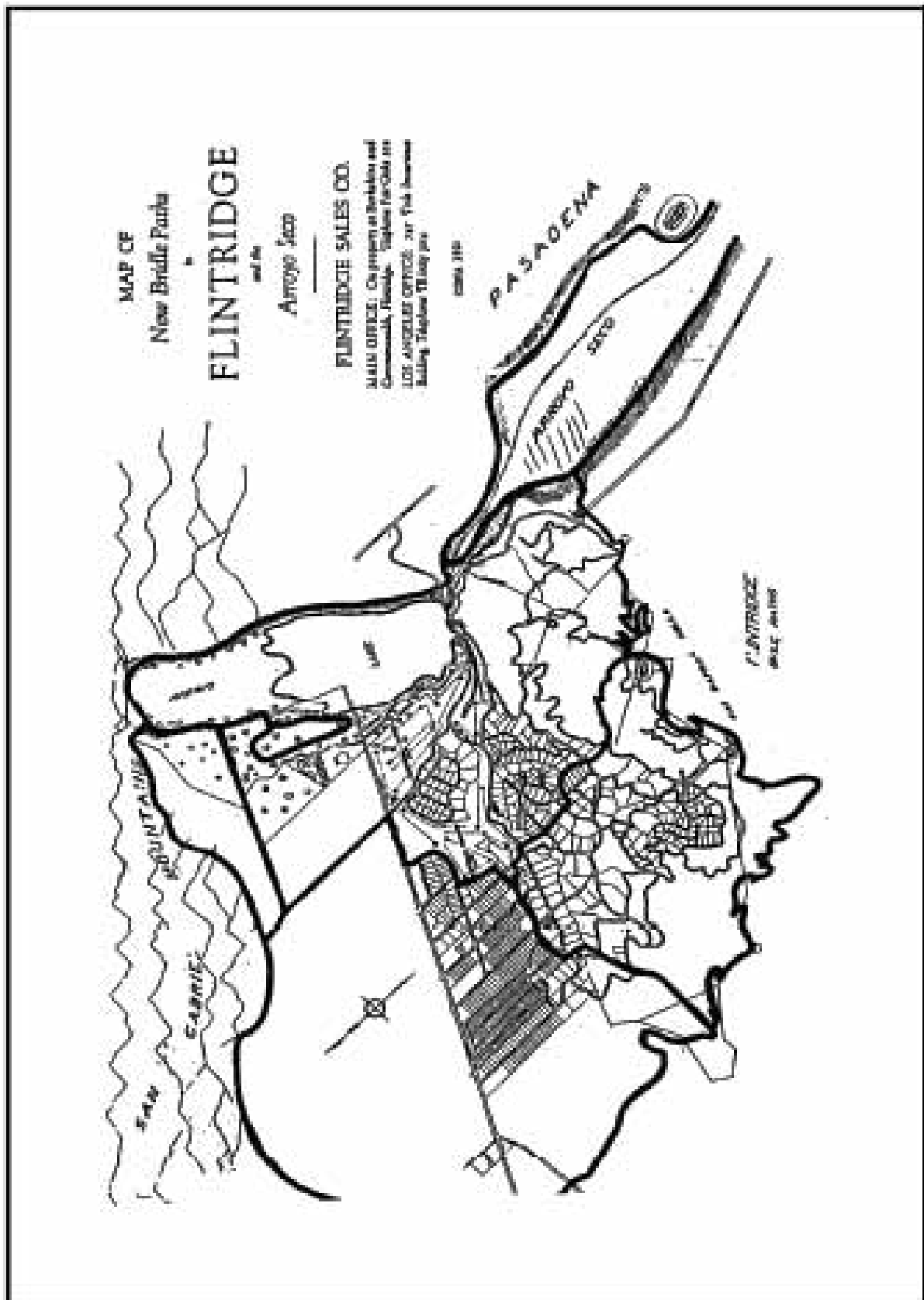
City staff serving as liaison to the Committee:

- Robert Stanley, Director of Community Development
- Steve Castellanos, Director of Public Works
- Ying Kwan, City Engineer
- Ann Wilson, Management Analyst

In constructing the Trails Master Plan, the Committee:

1. Was active in the purchase and development of the City’s Geographic Information System (GIS). This allowed volunteer Committee Members to map the trails for the first time using GPS technology. The GIS system has allowed the Committee to compile all available information about the trail system that exists within the City in one accurate, accessible place where it can be viewed by City staff reviewing development, thus assisting in protecting the trails. The mapping included descriptions of trail surfaces as they were walked, access points, etc. All of these have been mapped for the first time and will be available to the public.
2. Preliminary Title Reports regarding some properties within the City were obtained, and even further research on the properties adjoining the trails was conducted by volunteer members of the Committee.

3. The Committee was responsible for recommending to the City Council specific routes and properties for future trail development and for providing completion of the City's core circular trail as well as ultimately providing regional access to the City's trail system. With City Council direction, staff wrote a grant and obtained funds to finance part of the proposed trail segment. The Committee recommended to the City Manager and City Council that the complete set of future trails should be included in the City's Capital Improvements Budget, and the City Council approved that recommendation.
4. Members of the Committee worked closely with City staff, familiarizing them with trails issues, and leaving in place procedures to address those issues at the sunset of the Committee's duties.



GENERAL PLAN GOALS AND POLICIES RELATED TO TRAIL FACILITIES IN THE CITY OF LA CAÑADA FLINTRIDGE

LAND USE ELEMENT

- **Open Space/Parks and Recreation**
 - **Recreational Trails:** The City's trails serve as an important recreational resource and are historically a significant aspect of the community's rural atmosphere. The six acres of dedicated trail facilities are augmented by several miles of trail easements which connect the City to the Los Angeles County regional trail system, Pasadena and South Pasadena trails, and to the Gabrielino National Trail.

CIRCULATION ELEMENT

- **Trails System**
 - The La Cañada Flintridge community has a history of providing recreational access to the San Gabriel, San Rafael and Verdugo mountains through bridle paths and trails which have been continually developing since the early 1900's. While many of the trails located in the City are maintained under contract, the on-going upgrade and development of trails has been a priority to the City to provide access to publicly held open space and to enhance the overall quality of life for the community.

The La Cañada Flintridge Trails System Map, shown as Figure C-6, represents a compilation of several trails systems which are located within and around the City, including the U.S. Forest Service, the trails of the Arroyo Seco/Devils Gate area (also known as Hahamongna Watershed Park), the La Cañada Flintridge Trails Council, and the Santa Monica Mountains Conservancy.

These trails offer safe alternative routes for recreational pursuits and access to the community's natural resources. As such, the City recognizes this trail system as an important extension of the overall support systems provided by the community.

The system of recreational trails is incomplete, with several projects continuing to be planned and developed to link trails on the north and south sides of the City as well as improved connections to regional trails, including the Santa Monica Mountains Conservancy's Crosstown Trail, and the substantial network of trails in the Arroyo Seco and Pasadena areas.

- **Circulation Goals and Policies**
 - Circulation Element goals and policies define the City's vision for a balanced, efficient circulation system which incorporates many modes of travel and which allows for the safe movement of people and goods in and around La Cañada

Flintridge. These goals recognize the constraints posed by the existing built environment, but also capitalize upon the opportunities created by established transportation routes. Through these goal statements, the City also lends its support to regional, long-range efforts to manage congestion and reduce pollutant emissions within the South Coast Air Basin.

- **Alternative Modes of Transportation**

- ▲ **Goal 2:** Facilitate other modes of transportation, or alternatives to automobiles, including public transportation, bicycling, ridesharing and walking which support the land use plans and related transportation needs.

- **Trails System**

- Recreational Trails provide an important outlet and a safe alternative to the use of public streets for many recreational pursuits. Their continued maintenance and on-going development is important to the quality of life in La Cañada Flintridge.

- ▲ **Goal 8:** Preserve existing trails and promote coordinated and comprehensive trail systems for bicyclists, equestrians and hikers.

- ◆ **Policy 8.1:** Cooperate with public agencies, public utilities, and private organizations to maintain and keep in good repair all recreational trails, and seek to prevent loss of use or access to existing trails by encroachment or interference from abutting property owners.

- ◆ **Policy 8.2:** Continue to seek dedication of land and/or fees in lieu of such dedication for recreational trails as a requirement of new residential land divisions, where such dedications or fees have the opportunity to augment the existing trails system.

- ◆ **Policy 8.3:** Cooperate with public agencies, public utilities, and private organizations to promote the use and development of trails facilities on their land.

- ◆ **Policy 8.4:** The La Cañada Flintridge Trails System map...shall be maintained and updated as policies 8.2 and 8.3 above are implemented, and shall be made available for public reference and use. The Trail System shall be amended by separate resolution of the city Council as additional routes, trails and facilities are deemed appropriate.

- ◆ **Policy 8.5:** Maintain efforts to work with regional organizations, such as the Santa Monica Mountains Conservancy, to secure funding for the on-going implementation of the La Cañada Flintridge Trails System.

- ♦ **Policy 8.6:** Pursue alternative funding sources which support emerging recreational uses, such as bikeway monies, to provide a balanced recreational trail system to the community. Where appropriate, the trails will be identified as an adjunct of the Bikeway Plan.
- ♦ **Policy 8.7:** Install and maintain trail signage which promotes safety and courtesy between all trail users, including equestrians, hikers and bicyclists.

ENVIRONMENTAL RESOURCE MANAGEMENT ELEMENT

- **Goals**

- **Open Space and Recreation**

- ▲ **1.7:** Preserve and expand nonvehicular access to the Angeles National Forest trails and open lands remaining in the San Rafael Hills and San Gabriel Mountains. Encourage the dedication of additional lands to public open space use.
 - ▲ **1.8:** Encourage continued cooperation between public and private recreational services to assure a wide variety of recreational, educational and cultural programs for all segments of the community.
 - ▲ **1.9:** Preserve existing trails and promote coordinated and comprehensive trail systems for bicyclists, equestrians and hikers.
 - ▲ **2.2:** Parks, Trails and Recreation:
Public recreation facilities in La Cañada Flintridge consist of approximately two acres of park area; and approximately ten miles riding and hiking trails.

The City's riding and hiking trails are an important recreational resource and are historically a significant aspect of the rural atmosphere of the community. Parts of the trail system have existed intact for more than fifty years. The La Cañada Loop Trail is part of the Los Angeles County regional trail system. It connects to Pasadena and South Pasadena trails in the Arroyo Seco and to the Gabrielino National Recreational Trail in the Angeles National Forest. Two segments of the loop trail remain to be constructed. Local feeder trails need to be improved and, in some cases, reopened.

- ▲ **3.23:** Hiking and Equestrian Trails:
The system of existing hiking and equestrian trails is incomplete. The highest priority in completing the La Cañada Loop Trail is the extension of the Edison Right-of-Way easement from El Vago Street to the Arroyo Seco where it will connect with the Gabrielino National Recreational Trail

at the Gould Mesa picnic ground. The second priority in the Loop Trail System is the extension of the Edison Right-of-Way Trail southward to connect with the Descanso fire road. Efforts should be made to establish additional trail facilities which would provide desirable and logical connections to the trails network.

▲ **3.23: Motorcycles and Off-Road Vehicles:**

In isolated, uninhabited areas, the use of motorcycle “dirt bikes” on formal or informal motocross circuits is an appropriate form of recreational activity. However, in developed cities such as La Cañada Flintridge, motocross activity even in the relatively open hillside areas is unacceptable because of its potential for adverse environmental impacts, health and safety hazards, and audible nuisances. For these reasons, motocrossing shall be prohibited within City limits.

▲ **Public and Private Open Space:**

“Public” and “Semi-Public” open space uses consist of major utility rights-of-way, public parks and school playgrounds. Major utility rights-of-way in La Cañada Flintridge have also afforded an opportunity to extend the local trails system without disrupting residential property uses.

▲ **3.3.1: Cherry Canyon:**

The Cherry Canyon and adjoining Descanso hillsides together constitute a third of the southern border of the City and half of the prominent southern skyline. Consequently, these hillsides are of enormous scenic and aesthetic benefit to the City. Preservation of the Cherry Canyon area in its present state would ensure continued public access to and enjoyment of this valuable resource. The area is part of the north face of the San Rafael Hills, and is an excellent example of natural chaparral habitat on the north-facing slope. The San Rafael Hills are recognized as an area of regional significance and concern as an “important ecological area for chaparral habitat.”

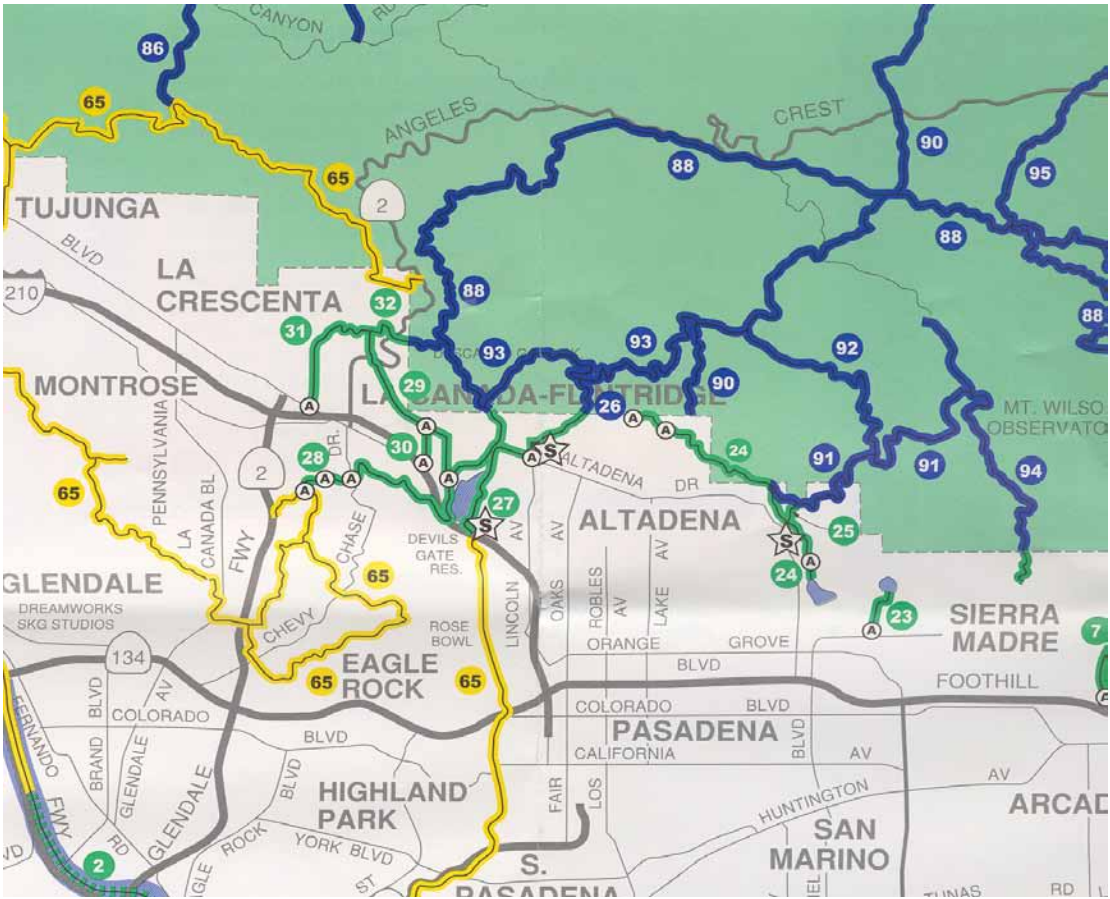
An important segment of the La Cañada Flintridge Loop Trail system passes through the Cherry Canyon area beginning with the Flint Wash trail at Hampstead Road and continuing up the canyon, turning northward on the Descanso fire road.

▲ **4.16: Publicly owned open space land shall be retained as such.** Reasonable efforts should be made to acquire large portions of privately owned hillside properties which contain significant community features and resources such as natural chaparral and wildlife habitat, areas of passive recreation, settings for riding and hiking trails and outdoor education, and other community-wide hillside amenities.

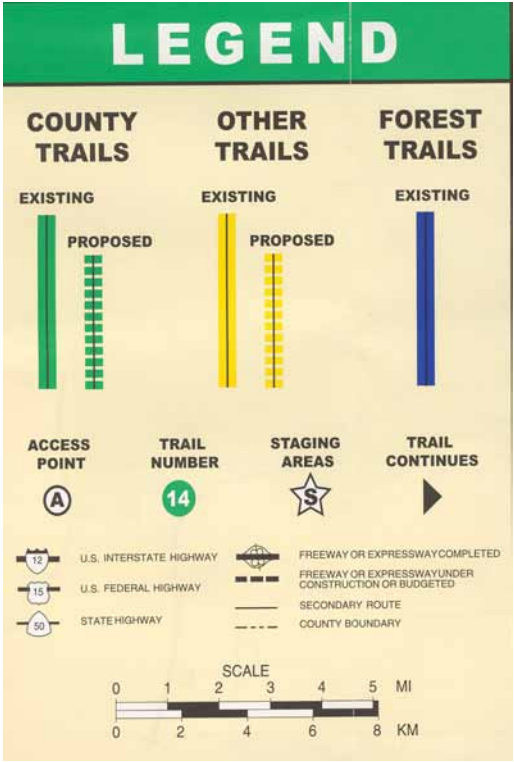
Open space areas of particular value include Cherry Canyon, Weber Canyon, Gould Canyon, Winery Canyon, Hall Beckley Canyon, Snover Canyon, Hay Canyon and their surrounding hillsides.

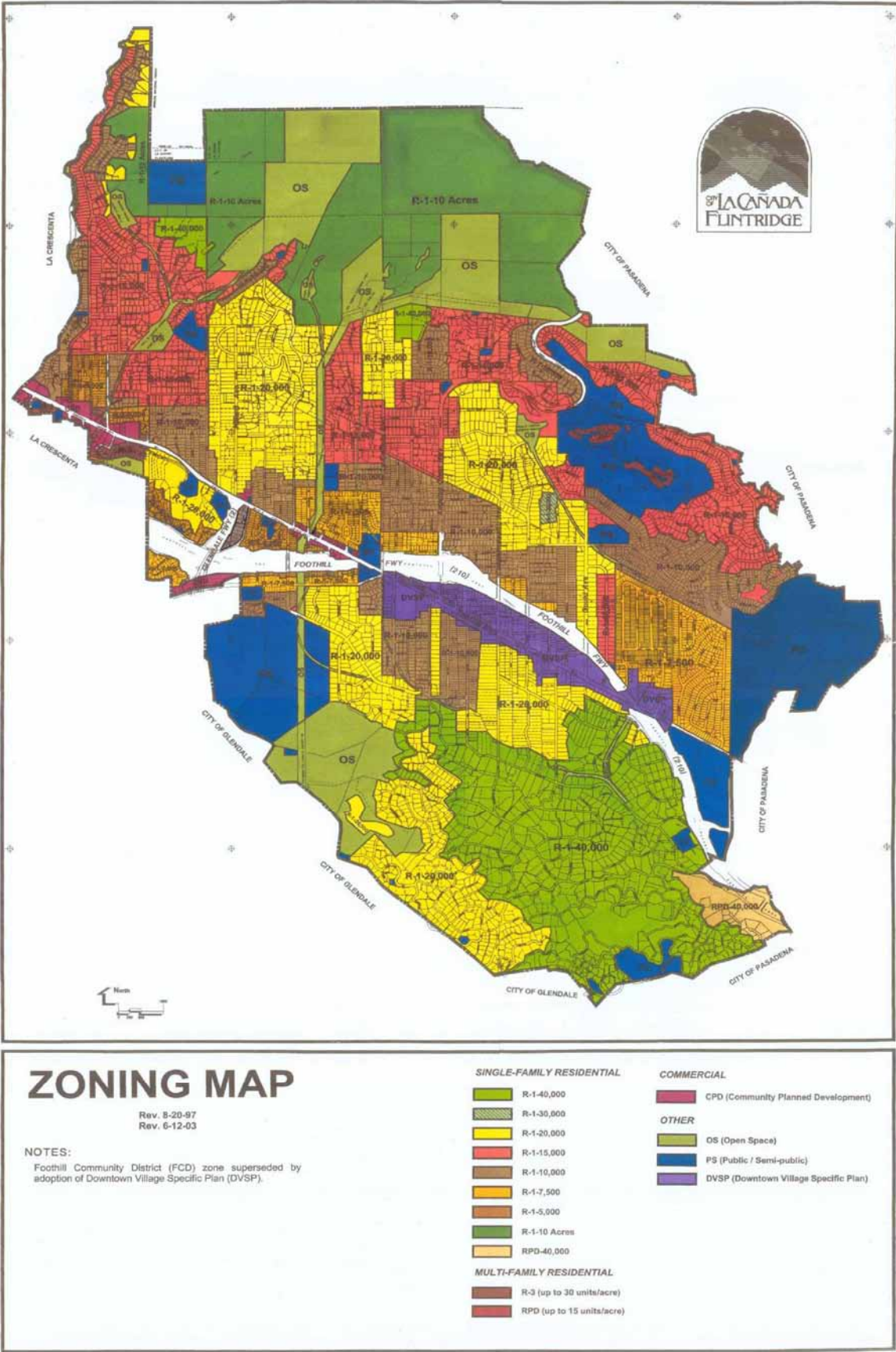
- ▲ **4.23:** The Environmental Resource Management Map...designates all existing hiking and equestrian trails and designates for dedication (acquisition) of proposed trails to complete the La Cañada Flintridge hiking and riding trail network.
- ▲ **4.24:** All publicly-owned lands committed to open space use or utility right-of-way use should be preserved as “open space”.
- ▲ **5.12:** The City shall adopt, pursuant to the Quimby Act, an ordinance which requires dedication of land or in-lieu fees for acquisition and development of local park facilities. Such ordinance shall achieve 3.5 acres of usable park site per 1,000 residents.

PORTIONS OF THE
LOS ANGELES COUNTY MASTER PLAN TRAILS MAP



TRAIL NUMBER		TRAIL LENGTH		TRAIL RATING: ND - Not Difficult, MD - Moderately Difficult		TRAIL NAME	
1	10.0	ND				Los Angeles River Trail	
2	16.0	ND				Los Angeles River Trail Extension	
3	2.0	ND				Palos Verdes Trail	
4	8.5	ND				Dominguez Channel Trail	
5	15.0	ND				Rio Hondo River Trail	
6	6.0	ND				Santa Anita Wash Trail	
7	2.0	ND				Santa Anita Wash Trail Extension	
8	24.0	ND				San Gabriel River Trail	
9	3.5	MD				Van Tassel Trail	
10	2.0	ND				San Jose Creek Trail	
11	28.0	MD				Schabarum Trail	
12	2.0	MD				7th Avenue Trail	
13	1.3	ND				Whittier Connector Trail	
14	9.0	D				Schabarum Extension Trail	
15	3.0	D				Schabarum Extension Connector Trail	
16	2.0	D				Schabarum Extension Connector Trail	
17	2.0	ND				Rowland Heights Connector Trail	
18	3.0	MD				Rowland Heights Loop Trail	
19	3.0	MD				Walnut Creek Park Trail	
20	10.0	MD				Frank G. Bonelli Regional Park Trail	
21	8.0	ND				Marshall Canyon Trail	
22	2.0	MD				Marshall Canyon Extension Trail	
23	1.0	ND				Hastings Debris Basin Trail	
24	7.0	D				Altadena Crest Trail	
25	1.0	D				Eaton Canyon Park Trail	
26	2.0	MD				Chaney/Sunset Ridge Trail	
27	3.0	ND				Devil's Gate-Arroyo Seco Trail	
28	3.0	ND				Flint Wash Trail	
29	2.3	ND				Gould Canyon Trail	
30	1.0	ND				Horse Lane	
31	4.0	D				La Canada Open Space Trail	
32	1.0	MD				La Canada Open Space Extension	
33	6.0	MD				San Gabriel River Trail to Mount Baldy	
34	7.0	D				Three Park Trail	
35	5.0	D				Trancas Canyon Trail	
36	20.0	D				Zuma Ridge Trail	
37	5.0	D				Zuma Ridge to Westlake Trail	





TRAILS INVENTORY



CERRO NEGRO TRAIL

trail

DESCRIPTION:	This trail winds around the upper reaches of Cherry Canyon, beginning just west of the Sugarloaf Drive gate and ending near the southern end of the Cherry Canyon Mtway. It is a scenic loop trail that has a picnic table and tie rail at its easternmost vista point. Both the Owl Trail and the Ultimate Destination trail can be reached from the Cerro Negro Trail.
DIFFICULTY/GAIN:	Moderately Difficult/ 245 ft. (1,575-1,820)
HISTORY:	Built in 1991. Designed by Ron Webster, built by Los Angeles Conservation Corps.
LENGTH:	1.16 miles
ACCESS POINTS:	Accessible from Hampstead Road, just south of Beresford Way and from the end of Sugarloaf Drive.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned by City.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by City.

Cerro Negro Trail (continued)

LOCATION (highlighted in red):





CHERRY CANYON FIRE/UTILITY ROADS

trail

DESCRIPTION:	Network of fire and utility roads in Cherry Canyon that connect the City's trails and also serve as trails in their own right. Their names, as indicated on the City's Geographic Information System (GIS) system are: The Descanso Fire Road, the Edison Road, the Cherry Canyon Fire Road and the Alpha Fire Road. There is a scenic overlook located in the northern section of Cherry Canyon called Five Points. This location provides a 360 degree view encompassing the San Gabriel Mountains to the ocean.
DIFFICULTY/GAIN:	Ranges from Not Difficult to Difficult/ 455 (1,385-1,840)
HISTORY:	Unknown
LENGTH:	3.05 miles
ACCESS POINTS:	West end of Hampstead Road, west end of Sugarloaf Drive, South end of Forest Hill Drive and the Stancrest Frontage Road in Glendale
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Mostly within the City of La Cañada Flintridge boundaries, but some portions are in the City of Glendale. Some portions are on County-owned property, some on Edison property, and some on City property. Southern California Edison has fee-owned property in the vicinity and has nonexclusive access road rights to get to its transmission facilities.
SURFACES:	Dirt
MAINTENANCE INFORMATION:	Maintained by various public agencies

Cherry Canyon Fire/Utility Roads (continued):

LOCATION (highlighted in red):





CONSERVANCY TRAIL

trail

DESCRIPTION:	Situated on the north-facing slopes of Cherry Canyon, the Conservancy Trail provides spectacular views of the San Gabriel Mountains as it winds through chaparral and oak woodland.
DIFFICULTY/GAIN:	Not Difficult/ 150 (1,465-1,615)
HISTORY:	Funded by the Santa Monica Mountains Conservancy, and the City of La Cañada Flintridge (50/50 match). The trail is named after the Santa Monica Mountains Conservancy. Dedicated January 22, 2005. Built using Los Angeles California Conservation Corps. Designed by Ron Webster.
LENGTH:	.47 miles
ACCESS POINTS:	Forest Hill Drive Gate, Edison Road
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned by the City.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by the City.

Conservancy Trail (continued):

LOCATION (highlighted in red):





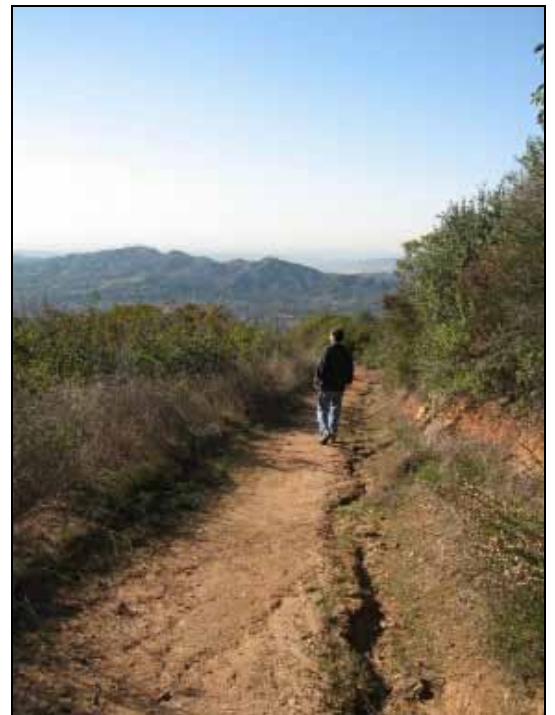
CROSS TOWN TRAIL

trail

DESCRIPTION:	Connects the Loop Trail and the Gould Canyon Trail. Referred to by the County of Los Angeles as the La Cañada Open Space Trail (Foothill Blvd. to Gould Canyon Trail) and the La Cañada Open Space Extension (from the Gould Canyon Trail east to the Angeles Crest Highway). The trail is wide and open with a gradual rise in elevation on the Edison property south of El Vago Street. Going north, the trail heads up into the foothills, affording spectacular views of the Crescenta-Cañada Valley, Cherry Canyon and the Verdugo Mountains.
DIFFICULTY/GAIN:	Moderately Difficult (to Gould) to Difficult (from Gould east)/1,125 feet (1,460-2,585)
HISTORY:	Portions of the trail north of El Vago were built by Los Angeles County in the early 1980s to connect already existing trail segments.
LENGTH:	3.13 miles
ACCESS POINTS:	Just north of Foothill Boulevard on Indiana Street, the north end of the Gould Canyon Trail above Harter Lane and east at Angeles Crest Highway north of Baytree Road.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	South of El Vago, the trail is on Edison property. North of El Vago, the trail is on Edison property, County property, City property and a 67-acre privately held parcel. The final segment of the trail is once again on Edison property as it reaches Angeles Crest Highway. Southern California Edison has been licensing its property on this trail north of Foothill Boulevard to the County of Los Angeles for trail use since the 1950s.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by the County of Los Angeles. Portions of this trail are under a license agreement with Southern California Edison, which owns those portions of the trail in fee. Utility roads maintained by Southern California Edison and Los Angeles County Flood Control.

Cross Town Trail (continued):

LOCATION (highlighted in red):





FLINT CANYON

trail

DESCRIPTION:

Trail runs through the Flintridge area, connecting Hahamongna Watershed Park in Pasadena on the east with the Cherry Canyon trails on the west. After passing under Oak Grove Drive and the 210 Freeway, the trail meanders through a lovely, undeveloped area with many oaks, sycamores and a year-round stream before coming out next to Berkshire Avenue. It then drops down to Woodfield Road, a private street, where it continues along the street, then crosses Chula Senda Lane where the trail surface becomes soft surface again. Parts of the trail widen to 15 to 25 feet or more. These green open spaces add interest and variety to the trail experience.

DIFFICULTY/GAIN:

Not Difficult/310 (1,050-1,360)

HISTORY:

Portions of the trail appear on Tract Map 9874, dated October 1, 1927. Another section appears on Tract Map 15002, dated April 14, 1948.

In the 1980s, a resident along the trail erected a gate and tried to close the trail. A decade-long effort led by the La Cañada Flintridge Trails Council and Equestrian Trails, Inc. was ultimately successful in keeping the trail open.

In 2003, the City purchased a half-acre of property from Los Angeles County (Flood Control Department) to ensure that this green open space with several large oaks would remain along the trail corridor.

The Santa Monica Mountains Conservancy has funded the repair of the eastern section of the trail, matching the City equally (50/50 match). Additionally, Proposition C Funds and Land and Water Conservation funds have been obtained for this repair.

LENGTH:

2.03 miles

Flint Canyon Trail (continued):

ACCESS POINTS: Accessible from: Hahamongna Watershed Park, Berkshire at Dover Road, Woodfield Road, Chula Senda west of Woodfield Road, Woodleigh Lane north of Berkshire Ave., Oakwood Ave. north of Berkshire Avenue, Commonwealth Avenue north of Berkshire Avenue, Beulah Dr. north of Berkshire Avenue, Berkshire Avenue west of Beulah Drive, Chevy Chase Drive north of Flintridge Avenue, and Hampstead Road – Cherry Canyon

AVAILABLE OWNERSHIP/EASEMENT INFORMATION: Ownership varies. Portions owned in fee by Los Angeles County. Easements held on private property by both Los Angeles County and the City.

SURFACES: Dirt/Decomposed Granite

MAINTENANCE INFORMATION: Maintained by the County of Los Angeles

LOCATION (highlighted in red):



Flint Canyon Trail (continued):





GATEWAY LINK TRAIL

trail (proposed)

DESCRIPTION:	In 2006, the City will design the Link District Linear Park. The project is to design a plan to move the traffic lanes to the north on Foothill Blvd. from the commercial district at Leata Lane to the YMCA, thus creating a greenbelt or park on the south side. There will be a meandering multi-modal trail within this greenbelt, extending the entire length of the greenbelt. It is desired that the Gateway Link Trail would connect with the proposed Rockridge Terrace Trail on the west in order to create a loop.
DIFFICULTY/GAIN:	Not Difficult
HISTORY:	This project is part of the Foothill Boulevard Specific Plan, adopted in 1991. The design for the Gateway Link has been budgeted and the project is currently in design.
LENGTH:	
ACCESS POINTS:	Leata Lane, Foothill Boulevard
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	City-owned
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	City will maintain this trail
APPROXIMATE LOCATION (highlighted in red):	





GEORGIAN SPUR

trail

DESCRIPTION:	Connects properties along Georgian Road and two flag lots behind Woodleigh Lane to the Flint Canyon Trail
DIFFICULTY/GAIN:	Not Difficult/25 (1,125 – 1,150)
HISTORY:	Built when Tract 13397 was built in 1945
LENGTH:	.18 miles
ACCESS POINTS:	Flint Canyon Trail at Chula Senda Lane
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Private ownership - easements along Georgian Road.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Not maintained by Los Angeles County as part of the official trail network.

LOCATION (highlighted in red):





GOULD CANYON

trail

DESCRIPTION:	Extends from Hahamongna Watershed Park in Pasadena on the south to the Cross Town trail north of Harter Lane. The trail provides a varied experience for the trail user because, in addition to passing between back yards, it winds by the scenic grounds of the Flintridge Riding Club, Jet Propulsion Laboratory, and through two large, undeveloped open space areas. From Canon de Paraiso to Green Lane, the trail runs along the County Flood Control channel. Before entering the forest, the trail is on Harter Lane for a short distance.
DIFFICULTY/GAIN:	Not Difficult to Difficult (varies)/950 feet (1,110-2,060)
HISTORY:	Severely damaged by storms, the trail was closed for approximately eight years. In 1996, the City, the Trails Council, the Santa Monica Mountains Conservancy and a local developer (who contributed funds and constructed a wall of gabion baskets to slow the force of water draining from his property above the trail) worked to reroute the trail. The Los Angeles Conservation Corps funded the necessary engineering.
LENGTH:	2.16 miles
ACCESS POINTS:	North side of Foothill Blvd., east of Viro Road; north end of Viro Road; east end of Stardust Road (lower); east end of Mero Lane; north end of Alminar Avenue; Middle of Canon de Paraiso; Stoneglen where it abuts the trail; Gould Avenue north of Venado Vista Drive; east end of Green Lane; Harter Lane east of Mountain Meadow Lane; north end of Harter Lane; Junction with the Cross Town Trail
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Ownership varies. Portions owned in fee by Los Angeles County. Easements held on private property by both Los Angeles County and the City of La Cañada Flintridge.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by the County of Los Angeles

Gould Canyon Trail (continued):

LOCATION (highlighted in red):





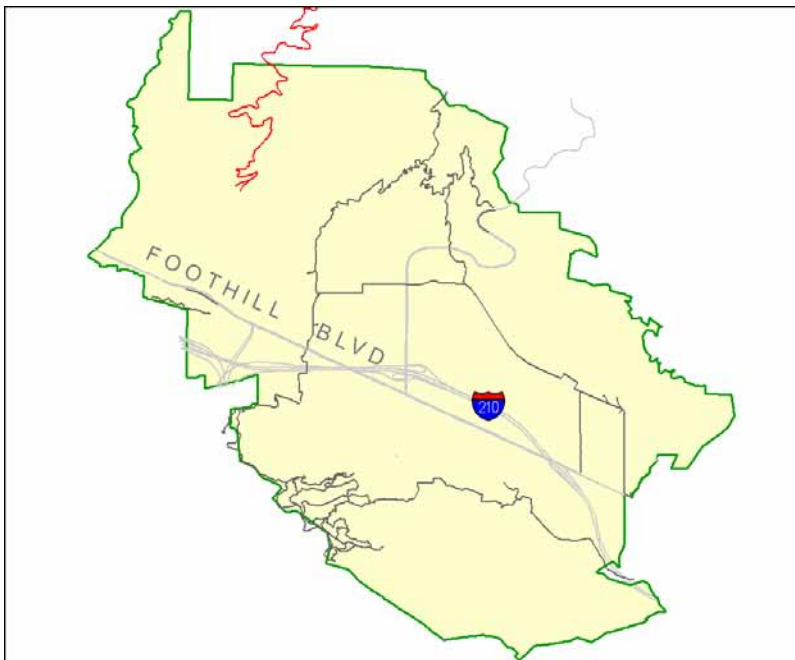
HALL BECKLEY CANYON/ EARL CANYON MTWY.

trail

DESCRIPTION:	This trail is primarily a fire road (Earl Canyon Mtwy.) that leads up into Angeles National Forest. The trail begins at the southern end, in the Hall Beckley Canyon Recreation Area, one of the City's few portals to the Angeles National Forest. The area is peaceful and rustic with many native sycamore trees.
DIFFICULTY/GAIN:	Difficult/2,025 (1,795-3,820)
HISTORY:	Threatened by development, Hall Beckley Canyon Recreation Area was purchased by the City with a \$600,000 grant from the Santa Monica Mountains Conservancy with a \$170,000 contribution by the City. Although the trail has existed for many years, the purchase of the recreation area saved the southern entrance of the trail. This area was dedicated on October 11, 2003.
LENGTH:	3.61 miles
ACCESS POINTS:	Accessible from La Sierra Drive on the southern end of the trail, and on the north from the Mount Lukens Fire Road.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	The southern end – Hall Beckley Canyon is owned by the City. Earl Canyon Mtwy. is located on property owned by Los Angeles County, the federal government and several privately held parcels.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Hall Beckley Canyon maintained by City. Earl Canyon Mtwy. maintained by Los Angeles County Fire Department

Hall Beckley Canyon/Earl Canyon Mtwy. (continued):

LOCATION (highlighted in red):





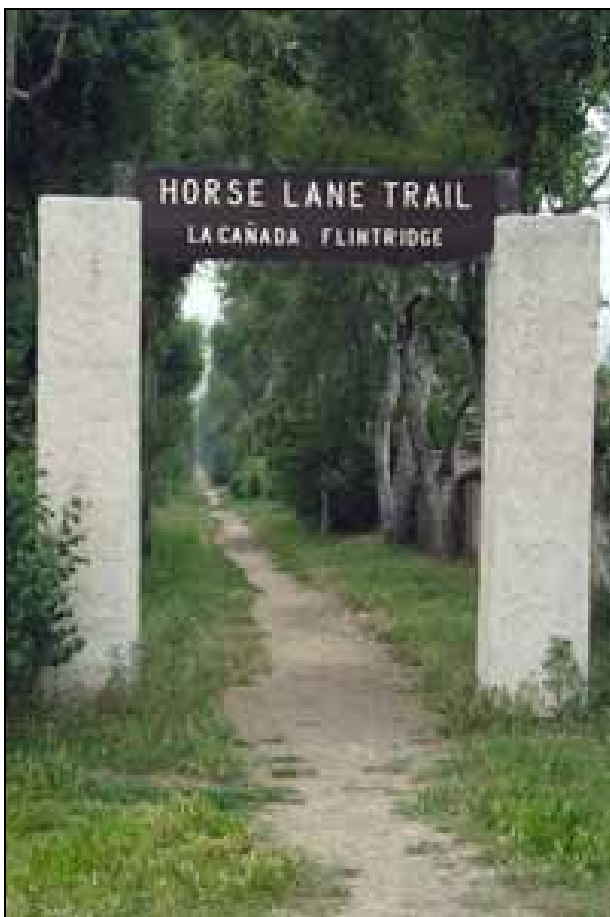
HORSE LANE

trail

DESCRIPTION:	Connects the Gould Canyon Trail south to Foothill Blvd. This trail is a beautiful, 25-foot wide trail, lined by eucalyptus, oaks and back yard vegetation. The planting at the Foothill Boulevard entrance was a joint project between La Cañada Valley Beautiful, the La Cañada Flintridge Trails Council and St. Bede's. It is maintained by La Cañada Valley Beautiful.
DIFFICULTY/GAIN:	Not Difficult/45 (1,195-1,240)
HISTORY:	Appears on Tract Map 3240, surveyed in 1920.
LENGTH:	.56 miles
ACCESS POINTS:	South entrance: north side of Foothill Blvd., between Crown Avenue and Alminar Avenue.; north Entrance: Alminar Avenue between Mero Lane and Revlon Drive.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned in fee by Los Angeles County
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by the County of Los Angeles

Horse Lane (continued):

LOCATION (highlighted in red):





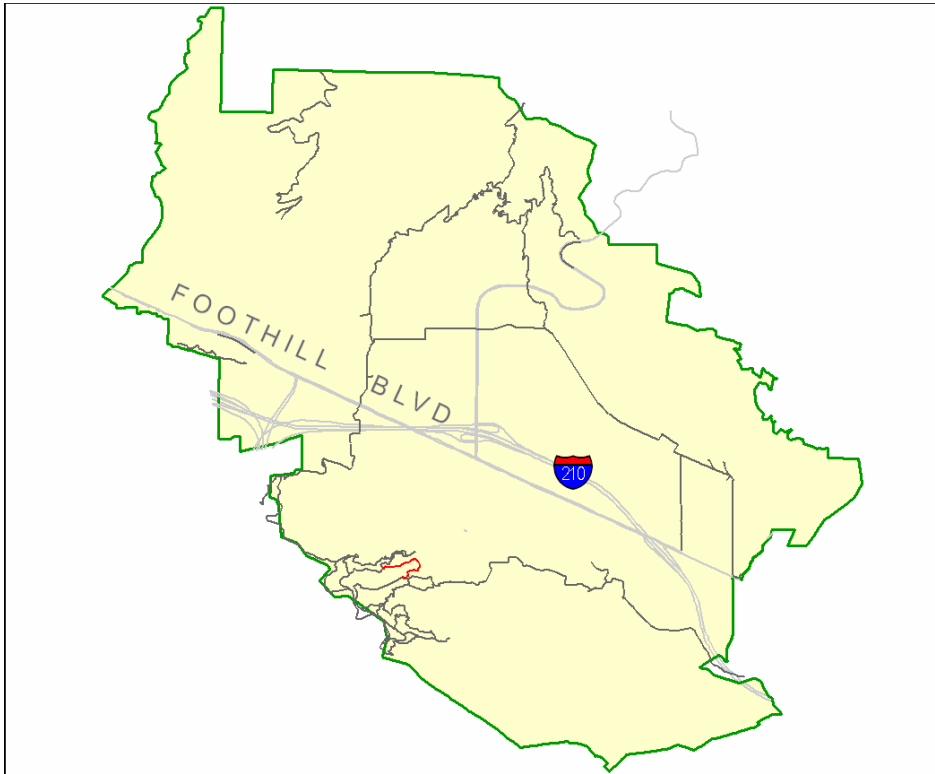
LIZ'S LOOP

trail

DESCRIPTION:	A user-friendly trail, connected by fire roads to the Flint Canyon Trail and the Conservancy Trail. The trail has beautiful views of the San Gabriel Mountains and the City.
DIFFICULTY/GAIN:	Not Difficult/95 (1,475-1,570)
HISTORY:	Built in 1994. Created with the help of Ron Webster, Jet Propulsion Lab's volunteer trail crew, and local residents. The trail is named in honor of long-time trails advocate and former President of the La Cañada Flintridge Trails Council, Liz Blackwelder.
LENGTH:	.38 miles
ACCESS POINTS:	Accessible from Flint Canyon Trail, Hampstead Road entrance just south of Beresford Way via the Cherry Canyon Mtway.; and from the Edison Road, which is accessed from the Forest Hill Gate south of Descanso Drive
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned by City.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by City.

Liz's Loop (continued):

LOCATION (highlighted in red):





LOOP TRAIL PHASE I

trail

DESCRIPTION:	Trail begins on Caltrans property near Indiana Avenue and Curran Street and extends southward across the 210 Freeway using an existing utility flume owned by Caltrans. It continues westward to the southern edge of the Caltrans property. This trail is the first of three phases in the completion of the “missing link” in the City’s core circular trail, envisioned in the City’s General Plan in 1979, following the City’s incorporation in 1976.
DIFFICULTY/GAIN:	Not Difficult
HISTORY:	Recreational Program Trail grant (\$236,000) obtained in 2005, with match from City (\$59,000). Construction to begin in 2006.
LENGTH:	.2 miles
ACCESS POINTS:	North entrance: Loop Trail Phase II – Edison Property at Indiana and Foothill Blvd. South entrance: Loop trail Phase II - Verdugo Blvd., between Alta Canyada Drive and Indiana Avenue.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned by Caltrans. Used by the City via a Memorandum of Understanding with Caltrans and Maintenance agreement for a twenty year period of time beginning in 2003.
SURFACES:	Dirt, Concrete coated with S-9300 Bond-Kote (by Super-Krete, Inc.) – creates slip-resistant and abrasion-resistant surface on existing older concrete (flume).
MAINTENANCE INFORMATION:	Maintenance Agreement executed with Caltrans for twenty-year period of time beginning in 2003. Caltrans encroachment permit obtained for construction.

Loop Trail Phase I (continued):

LOCATION (highlighted in red):





LOOP TRAIL PHASE II

trail (proposed)

DESCRIPTION:

Proposed. This phase would begin on Southern California Edison property, at Foothill Boulevard near Indiana Avenue. From there, it proceeds south across Edison property to be licensed by the City to connect with the northern point of Loop Trail Phase I. Another segment begins at the southern edge of the Caltrans property on the southern part of Loop Trail Phase I. From there, it will proceed south to the west side of the Edison property. It would cross Verdugo Boulevard with an in-pavement lighted crosswalk. It would proceed southward along Edison property, cross the County-owned Winery Channel and continue southward to the edge of Descanso Drive. This trail is the second of three phases in the completion of the “missing link” in the City’s core circular trail, envisioned in the City’s General Plan in 1979, following the City’s incorporation in 1976.

DIFFICULTY/GAIN:

Not difficult

HISTORY:

The City has an existing license agreement with Edison for the trail from Foothill Blvd. south to the Caltrans property (Phase I). The City expects to enter into a license agreement for the remainder of the trail in 2006.

LENGTH:

Approximately one-third mile

ACCESS POINTS:

Foothill Blvd. at Indiana Avenue (north); Verdugo Blvd. east of Alta Canyada Blvd., Descanso Drive (north side) near Descanso Gardens

AVAILABLE
OWNERSHIP/EASEMENT
INFORMATION:

Edison owned; City licensed.

SURFACES:

Dirt, Stamped concrete, Decomposed granite

MAINTENANCE
INFORMATION:

To be maintained by the City through a license agreement with Southern California Edison.

Loop Trail Phase II (continued):

APPROXIMATE LOCATION (highlighted in red):





LOOP TRAIL PHASE III

trail (proposed)

DESCRIPTION:	Descanso Boulevard through western side of Descanso Gardens, on property owned by Los Angeles County to existing Fire Road. If constructed, this trail would be the third of three phases in the completion of the “missing link” in the City’s core circular trail, envisioned in the City’s General Plan in 1979, following the City’s incorporation in 1976.
DIFFICULTY/GAIN:	Moderately Difficult to Difficult
HISTORY:	n/a
LENGTH:	Approximately .5 mile
ACCESS POINTS:	TBD
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned by County of Los Angeles and used by Descanso Gardens by Agreement with the County of Los Angeles.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Would be maintained by the City through agreement with Los Angeles County.

Loop Trail Phase III (continued):

APPROXIMATE LOCATION (highlighted in red):





LUKENS CONNECTION

trail

DESCRIPTION:	Short, steep trail that follows the ridgeline from the highest point of the Cross Town Trail to the Mount Lukens Fire Road. Provides one of the two northern connections into the Angeles National Forest. There is a picnic table at the juncture of the Cross Town Trail and the Lukens Connection.
DIFFICULTY/GAIN:	Difficult/690 (2,575-3,265)
HISTORY:	Built by Los Angeles County
LENGTH:	.72 miles
ACCESS POINTS:	The Cross Town Trail to the south and the Mount Lukens Fire Road to the north.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	On City property and private property
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Although the Lukens Connection was built by Los Angeles County, it is not considered part of the County's official trail network and is not maintained by the County. Volunteer trail users have performed extensive maintenance duties on this trail.

Lukens Connection (continued):

LOCATION (highlighted in red):





OWL TRAIL

trail

DESCRIPTION:	In Cherry Canyon - connects Flint Canyon Trail. Going south, it crosses the Cerro Negro Trail and connects to the Fire Road. The lower portion of the trail is a beautiful, shady trail set among oaks near a year-round stream. The upper portion is steep and rocky.
DIFFICULTY/GAIN:	Moderately Difficult/390 (1,365-1,755)
HISTORY:	The upper portion of the trail is an unofficial trail cut by trail users.
LENGTH:	.53 miles
ACCESS POINTS:	Hampstead Road entrance to Cherry Canyon. Enter at south end from the Cerro Negro Trail.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned by the City
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by the City

Owl Trail (continued):

LOCATION (highlighted in red):





ROCKRIDGE TERRACE

trail (proposed)

DESCRIPTION:	Proposed Trail. It is desired that the Gateway Link Trail would connect with the proposed Rockridge Terrace Trail on the west in order to create a loop.
DIFFICULTY/GAIN:	Unknown
HISTORY:	The City recently purchased the Rockridge Terrace property, and is using it as a conservation area. It is currently proposed that a trail extend from the Rockridge Terrace to the YMCA. Easements would be needed from some property owners to build the full trail. Even if the full trail is not built, the City plans to construct a trail in the Rockridge Terrace Conservation Area.
LENGTH:	Unknown
ACCESS POINTS:	Rockridge Terrace and the Crescenta-Cañada YMCA
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	The City owns the Rockridge Terrace property (approximately 5.5 acres total). If easements are acquired, they will be held by the City.
SURFACES:	Anticipated Dirt/Decomposed granite
MAINTENANCE INFORMATION:	It is planned that the City will maintain the trail.

APPROXIMATE LOCATION (highlighted in red):





ULTIMATE DESTINATION

trail

DESCRIPTION:	With sweeping views of the San Gabriel Mountains, the trail is located on the upper slopes of Cherry Canyon. At the Ultimate Destination vista point there are two picnic tables and a hitching rail.
DIFFICULTY/GAIN:	Not Difficult/190 (1,630-1,820)
HISTORY:	Funded by the Santa Monica Mountains Conservancy and the City in a 50/50 match, this trail was built in 2002. It was dedicated on April 18, 2002.
LENGTH:	.62 miles
ACCESS POINTS:	Accessible from Cerro Negro Trail, and the fire road in Cherry Canyon, near Lookout Tower.
AVAILABLE OWNERSHIP/EASEMENT INFORMATION:	Owned by the City.
SURFACES:	Dirt/Decomposed Granite
MAINTENANCE INFORMATION:	Maintained by the City.

Ultimate Destination Trail (continued):

LOCATION (highlighted in red):





TRAILS – DESIRED IMPROVEMENTS

TRAIL	DESIRED IMPROVEMENTS
CERRO NEGRO	Damage caused by the winter rains of 2004-05 needs to be repaired. The easternmost section has not been maintained and needs to be renovated.
CHERRY CANYON FIRE/UTILITY ROADS	None
CONSERVANCY TRAIL	Frequent water runoff maintenance is necessary.
CROSS TOWN TRAIL	A multiple switchback area on the western portion of the trail needs to be reinforced to prevent landslides onto the trail.
FLINT CANYON	Extensive repairs need to be done on the eastern section of the trail where it has been severely eroded. Funds are available for this repair, and the repair will take place in the near future. Additionally, the rains of 2004-05 created the following issues: The trail under the Berkshire Place bridge is now impassable for horses and must be replaced. The final leg of the trail, before it reaches Cherry Canyon, was damaged and now needs a major restoration. It would be desirable to extend the trail along the Flint Canyon Flood Control channel east of Woodleigh as an additional trail segment.
GATEWAY LINK TRAIL	Construction of trail.
GEORGIAN SPUR	Should be incorporated into the Los Angeles County official trail network or should be deeded to the City.
GOULD CANYON	The section of the trail from Green Lane to Harter Lane was severely damaged in the winter rains of 2004-05. The trail is currently impassable for horses. In addition to the trail repair, a solution needs to be found for water flowing off Angeles Crest Highway and undermining the trail each year. Consider improving the existing easement north.

Desired Trail Improvements (continued)

HALL BECKLEY CANYON/EARL CANYON MTWY.	None
HORSE LANE	None
LIZ'S LOOP	Frequent water runoff maintenance is necessary.
LOOP TRAIL PHASE I	None
LOOP TRAIL PHASE II	Construction of trail.
LOOP TRAIL PHASE III	Construction of trail.
LUKENS CONNECTION	Incorporation into the County's official trail network.
OWL TRAIL	The upper portion of the trail is rutted and in need of repair/reconfiguration. The portion between the Cerro Negro Trail and the Alpha Fire Road needs to be rebuilt/reconfigured.
ROCKRIDGE TERRACE	Construction of trail.
ULTIMATE DESTINATION	Ultimate Destination Pocket Park is funded in the City's 2005-06 Budget and is to be constructed along the Ultimate Destination Trail. This project includes trees.

CITY OF LA CAÑADA FLINTRIDGE
COMMUNITY DEVELOPMENT DEPARTMENT
PROJECT REVIEW PROCEDURE: TRAILS

1. Planner reviews trails map on Assessor's Map, GIS map (on the internet) and determines whether the project or project-related property is part of, on or adjacent to a trail, staging area, trailhead, entryway, proposed trail, or dedicated but unused trail (trails-affected).
2. Planner determines whether the project potentially impacts the trail or trail easement.
3. If property is trails-affected and the project potentially impacts the trail or trail easement, the Planner checks all information on the GIS and elsewhere regarding the property (AP maps, appended title reports, tract map, and further research if necessary) to determine the level of documentation that exists regarding the potential trail. Once the Planner determines the Tract Map number or the Parcel Map number, he/she will check the more detailed information located in the Planning Department. The Planner will determine if there is a trail, trail easement, or proposed trail on the property.
4. If property is trails-affected, all permits and project approvals shall be consistent with the protection and preservation of all existing and proposed trails and trail rights identified in the Trails Master Plan. The Community Development Department shall impose, or recommend, the imposition of all permit conditions necessary to carry out the purposes of the Trails Master Plan. If the GIS system database for the property says "See Community Development Director", the Planner will also refer the project to the Community Development Director and will notify the Trails Council.
5. If the project is ministerial, the property has been determined to be trails-affected, and the documentation establishing the location and nature of the trail is incomplete or ambiguous, then, where appropriate and in consultation with the City Manager and City Attorney, the Community Development Director will pursue negotiations with the property owner to acquire an appropriately documented and recorded easement or other property right to preserve the trail. As needed, the Community Development Director shall consult the City Manager, to consider appropriate action to acquire the necessary rights to preserve the trail or proposed trail, or establish rights for a new trail.
6. If the project is discretionary, the property has been determined to be trails-affected, and the documentation establishing the location and nature of the trail is incomplete or ambiguous, the Community Development Director, in consultation with the City Manager and City Attorney, will determine if there is a nexus between the impact of the proposed development and the trail or proposed trail. No precise mathematical calculation is necessary to show the required reasonable relationship, but the City must make an individualized determination that the required dedication is related, both in nature and extent, to the impact of the proposed development – i.e. that it is roughly proportional. If such a nexus is found, then the Director will condition the approval of the project in such a manner to protect the trail. If a survey is necessary, the developer will be required to pay for the survey to ensure the easement and trail is located appropriately. In the event there is no nexus, then, where appropriate and in consultation with the City Manager and City Attorney, the Community Development Director may pursue negotiations with the property owner to acquire an appropriately documented and recorded easement or other property right to preserve the trail. As needed, the

Community Development Director shall consult the City Manager, to consider appropriate action to acquire the necessary rights to preserve the trail, or establish rights for a new trail.

7. If reconstruction of a trail is necessary for any reason, following the construction of the project, the Planner shall require the developer to adhere to the County Guidelines for trails standards, and/or the City design guidelines as indicated in the Trail Master Plan, where appropriate as determined by the Community Development Director.

This procedure is subject to change, as needed, at the discretion of the Community Development Director.

TRAILS ORDINANCE

ORDINANCE NO. 355

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF LA CAÑADA FLINTRIDGE ADDING CHAPTER 4.64 TO THE LA CAÑADA FLINTRIDGE MUNICIPAL CODE PERTAINING TO TRAILS

RECITALS

WHEREAS, the City Council of the City of La Cañada Flintridge finds and determines that reasonable rules, regulations, restrictions and requirements on the use of Trails within the City are necessary in order to protect recreational and other opportunities as well as to protect the health, safety and/or general welfare of the community;

NOW, THEREFORE, the City Council of the City of La Cañada Flintridge does hereby ordain as follows:

SECTION 1: Title 4 of the La Cañada Flintridge Municipal Code is hereby amended by adding thereto Chapter 4.64 to read as follows:

Chapter 4.64 Trails

- 4.64.010 Definitions.**
- 4.64.020 Applicability.**
- 4.64.030 Acts Prohibited.**
- 4.64.040 Penalty.**

4.64.010 Definitions. For purposes of this chapter, the following words, terms, and/or phrases shall be defined as follows:

“Trail(s)” means any pathway, avenue, course, accessway or route, improved or unimproved, commonly used for hiking, walking, horseback riding or other recreational purpose and identified as a “trail” on the City Trail Map, as may be amended from time to time. Trail(s) includes both City-Owned Trails and Non-City-Owned Trails, as defined herein.

“City-Owned Trail(s)” means a Trail owned by, or dedicated and accepted by, the City of La Cañada Flintridge, or a trail located on an easement that has been accepted by the City. City-Owned Trails are herein designated as a City “Park/Recreational Facility” as provided under this Code.

“Non-City-Owned Trail(s)” means a Trail which is not a City-Owned Trail.

“City Official” means the City Manager, Director of Community Development or the Director of Public Works, or their respective agents, deputies or designees.

“Person” means an individual or entity (excluding public utilities or governmental agencies or their employees and agents including, but not limited to, public safety and maintenance personnel while engaged in official duties on behalf of the utility or agency).

4.64.020 Applicability. To the extent allowed by law, this ordinance shall apply to all Trails within the boundaries of the City: (a) with respect to City-Owned Trails, Section 4.64.030 (A) shall apply; (b) with respect to Non-City-Owned Trails, Section 4.64.030 (B) shall apply; and with respect to all persons owning or controlling property abutting or adjoining any Trail, Section 4.64.030 (C) shall apply.

4.64.030 Acts Prohibited. It shall be unlawful for any person to engage in the following conduct or activity:

A. Conduct on a City-Owned Trail:

1. No person shall start or maintain a fire on a City-Owned Trail.
2. No person shall smoke or discard any lighted match, lighter, or tobacco product, or any other burning or combustible material, on a City-Owned Trail.
3. No person shall place any solid or liquid waste on a City-Owned Trail without first obtaining written permission from a City Official.
4. No person shall drive or operate motorized vehicles including, but not limited to, cars, trucks, motorcycles, motorbikes, go-carts, all-terrain vehicles, Bobcats, tractors, scooters with a motor, or electric vehicles, on a City-Owned Trail without first obtaining written permission from a City Official (wheelchairs, authorized maintenance or utility vehicles, and emergency vehicles are exempt from this subsection.)
5. No person shall dig or plow on any portion of a City-Owned Trail, in any manner, without first obtaining the written permission from a City Official.
6. No person shall place, or allow to remain in place, any litter or debris, including, but not limited to, non-equine waste and/or construction materials, generated directly or indirectly by that person's use of such City-Owned Trail.
7. No person shall obstruct any portion of a City-Owned Trail in any manner including, but not limited to, the use of materials, plants, signs, or vehicles.
8. No person shall use a City-Owned Trail for the purpose of obtaining ingress and/or egress to/from his/her property for construction purposes without first obtaining the written permission from a City Official (unless such access is specifically granted to such person pursuant to the documents creating the City-Owned Trail.)
9. No person shall place or erect any structure on a City-Owned Trail without first obtaining written permission from a City Official.
10. No person shall plant oleander, yew, cacti, castor bean plant or any poisonous or potentially harmful species, on or within five (5) feet of a City-Owned Trail.

11. No person may bring a dog and/or cat onto a City-Owned Trail unless such dog and/or cat is kept on a leash or chain not longer than six (6) feet in length and is kept under full control of its owner or custodian.
12. No person shall place or erect a sign or placard on a City-Owned Trail without first obtaining written approval from a City Official.
13. No person shall mark, disfigure, destroy, damage, remove or tamper with any trail sign located on a City-Owned Trail.
14. No person shall possess, consume or distribute alcohol or an open container of alcohol on a City-Owned Trail except with a permit issued pursuant to Chapter 4.05 of this Code.
15. No person shall fail to yield on a City-Owned Trail as follows: pedestrians, including hikers, shall yield the right-of-way to equestrians. Bicyclists shall yield the right-of-way to equestrians and pedestrians, including hikers.
16. No person shall operate a bicycle or ride a horse in a negligent, unsafe or reckless manner such that his/her conduct would endanger any person or property on a City-Owned Trail.
17. No person shall possess, shoot, fire, discharge, or cause to be possessed, shot, fired or discharged, any rifle, shotgun, pistol, revolver, firearm, or other device designed or intended to discharge, or capable of discharging, any missile, cartridge, shell, ammunition, paintball, pellet, BB, or other device capable of penetrating human epidermal, on a City-Owned Trail.

B. Conduct on a Non-City-Owned Trail:

1. No person shall start or maintain a fire on a Non-City-Owned Trail.
2. No person shall smoke or discard any lighted match, lighter or tobacco product, or any other burning or combustible material, on a Non-City-Owned Trail.
3. No person shall place any solid or liquid waste on a Non-City-Owned Trail without first obtaining written permission from the property owner of such Non-City-Owned Trail.
4. No person shall drive or operate motorized vehicles including, but not limited to, cars, trucks, motorcycles, motorbikes, go-carts, all-terrain vehicles, Bobcats, tractors, scooters with a motor, or electric vehicles on a Non-City-Owned Trail without first obtaining written permission from the property owner of such Non-City-Owned Trail (wheelchairs, authorized maintenance or utility vehicles, and emergency vehicles are exempt from this subsection.)

5. No person shall dig or plow on any portion of a Non-City-Owned Trail, in any manner, without first obtaining written permission from the owner of such Non-City-Owned Trail.
6. No person shall place, or allow to remain in place, any litter or debris, including, but not limited to, non-equine waste and/or construction materials, generated directly or indirectly by that person's use of such Non-City-Owned Trail.
7. No person shall obstruct any portion of a Non-City-Owned Trail in any manner including, but not limited to, the use of materials, plants, signs, or vehicles.
8. No person shall use a Non-City-Owned Trail for the purpose of obtaining ingress and/or egress to/from his/her property for construction purposes without first obtaining written permission from the owner of such Non-City-Owned Trail (unless such access is specifically granted to such person pursuant to the documents creating the Non-City-Owned Trail.)
9. No person shall place or erect any structure on a Non-City-Owned Trail without first obtaining written permission from the owner of such Non-City-Owned Trail.
10. No person shall plant oleander, yew, cacti, castor bean plant or any poisonous or potentially harmful species, on or within five (5) feet of a Non-City-Owned Trail.
11. No person may bring a dog and/or cat onto a Non-City-Owned Trail unless such conduct is expressly permitted by the owner of such Non-City-Owned Trail and the dog and/or cat is kept on a leash or chain not longer than six (6) feet in length and such dog and/or cat is kept under full control of its owner or custodian.
12. No person shall place or erect a sign or placard on a Non-City-Owned Trail without first obtaining written approval from the owner of such non-City-Owned Trail.
13. No person shall mark, disfigure, destroy, damage, remove or tamper with any trail sign located on a Non-City-Owned Trail.
14. No person shall possess, consume or distribute alcohol or an open container of alcohol on a Non-City-Owned Trail except with a permit issued pursuant to Chapter 4.05 of this Code.
15. No person shall fail to yield on a Non-City-Owned Trail as follows: pedestrians, including hikers, shall yield the right-of-way to equestrians. Bicyclists shall yield the right-of-way to equestrians and pedestrians, including hikers.

16. No person shall operate a bicycle or ride a horse in a negligent, unsafe or reckless manner such that his/her conduct would endanger any person or property on a Non-City-Owned Trail.
17. No person shall possess, shoot, fire, discharge, or cause to be possessed, shot, fired or discharged, any rifle, shotgun, pistol, revolver, firearm, or other device designed or intended to discharge, or capable of discharging, any missile, cartridge, shell, ammunition, paintball, pellet, BB, or other device capable of penetrating human epidermal, on a Non-City-Owned Trail.

C. Conduct on Property Adjoining and Abutting Trails:

1. No person shall plant oleander, yew, cacti, castor bean, plant or any poisonous or potentially harmful species, on or within five (5) feet of a Trail.
2. No person shall allow his/her trees (except where the tree is a protected species as identified in Chapter 4.26 of this Code) to encroach upon the free use and enjoyment of a Trail including, but not limited to, allowing trees or vegetation to overhang a Trail below a height of ten (10) feet over the Trail bed.
3. No person shall allow his/her trees or vegetation to block or impinge upon reasonable line-of-sight views of Trail users including, but not limited to, equestrians on horseback who are crossing a street or roadway. Clearance levels shall be determined by a City Official.
4. No person shall drain, or allow the flow of water from, a pool, spa, shower, bath, kitchen, laundry, bathroom, or any form of sewage, onto a Trail. No person shall build, maintain, or use a pipe, drain or artificial device, or regrade their property in such a manner as to result in a concentrated or non-sheet flow of surface water including, but not limited to, landscape and irrigation system drainage, onto a Trail. Nothing in this Chapter shall prohibit the non-concentrated draining of storm water from adjoining property onto a Trail provided that such flow has not been artificially diverted onto the Trail.
5. No person shall install any type or kind of irrigation system, other than drip irrigation, within five (5) feet of a Trail.
6. No person shall allow sprinklers or irrigation systems to spray, seep, or flow onto a Trail.
7. Notwithstanding any provision of the City's Building or Zoning Codes, no person shall erect or modify a fence or wall within five (5) feet of a Trail, regardless of the height of said fence or wall, without having first obtained a building permit from the Building and Safety Division. No building permit shall be issued for a fence which is determined by a City Official to be hazardous to Trail users or horses on the Trail, including, without limitation, barbed wire fencing and sharp or pointed wrought iron fencing. No fence or wall located within five (5) feet of a Trail shall be covered with fabric, wood sheeting, or plastic sheeting unless expressly approved by a City Official.

4.64.040 Penalty.**A. Misdemeanor and Infraction Characterization:**

1. Violation of Section 4.64.030 (A) and/or (B), subsections 1, 5, 7, 9, 10, 12, 13, 14, 17, and/or (C) 1, 2, 3, 4, 5 and/or 7, shall constitute a misdemeanor.
2. Violation of Section 4.64.030 (A) and/or (B), subsections 2, 3, 4, 6, 8, 11, 15, 16, and/or (C) 6, shall constitute an infraction.
3. Violation of Section 4.64.030 (A) and/or (B), subsection 3, shall constitute a misdemeanor if the solid waste involved is more than three (3) cubic feet in size or the liquid waste involved is more than one (1) gallon.
4. Violation of Section 4.64.030 (A) and/or (B), subsection 7, shall constitute an infraction if the obstruction involved is in place for less than three (3) hours.
5. Violation of Section 4.64.030 (A) and/or (B), subsection 8, shall constitute a misdemeanor for the fourth and subsequent violation(s).
6. Violation of Section 4.64.030 (A) and/or (B), subsection 12, shall constitute an infraction if the sign or placard is temporary and made of paper or cardboard.

B. Penalty.

1. A misdemeanor under this chapter shall be punishable as provided under this Code.
2. An infraction under this chapter shall be punishable as provided under this Code, however, any person who has committed seven (7) violations of this chapter, regardless of the amount of time between such violations, shall be guilty of a misdemeanor for each and every subsequent violation and may be punished as such as provided under this Code.

C. Prosecutorial Discretion.

Any offense that is designated a misdemeanor under this chapter may be prosecuted as an infraction provided that the prosecuting attorney files a complaint charging the offense as an infraction and the defendant, at the time of arraignment, does not object to having the case proceed as an infraction.

SECTION 2: Publication

This Ordinance shall take effect thirty (30) days after its final passage and, within fifteen (15) days after its passage, the City Clerk shall cause it to be published in the La Cañada Valley Sun, a newspaper of general circulation in the City of La Cañada Flintridge and hereby designated for that purpose.

SECTION 3. Severability

If any provision of this ordinance as herein enacted or hereafter amended, or the application thereof to any person or circumstance, is held to be invalid, such invalidity shall not affect the other provisions or applications of this ordinance (or any section or portion of section hereof), which can be given effect without the invalid provision or application and, to this end, the provisions of this ordinance are, and are intended to be, severable.

PASSED, APPROVED, and ADOPTED this 3rd day of April, 2006.

Anthony J. Portantino, Mayor

ATTEST:

Kathleen R. Sessman, City Clerk

State of California)
County of Los Angeles) ss.
City of La Cañada Flintridge)

I, Kathleen R. Sessman, City Clerk of the City of La Cañada Flintridge, California, DO HEREBY CERTIFY that the foregoing Ordinance No. 355 was introduced for first reading on March 20, 2006. Thereafter, said Ordinance was duly approved and adopted at a regular meeting of the City Council on April 3, 2006, by the following vote:

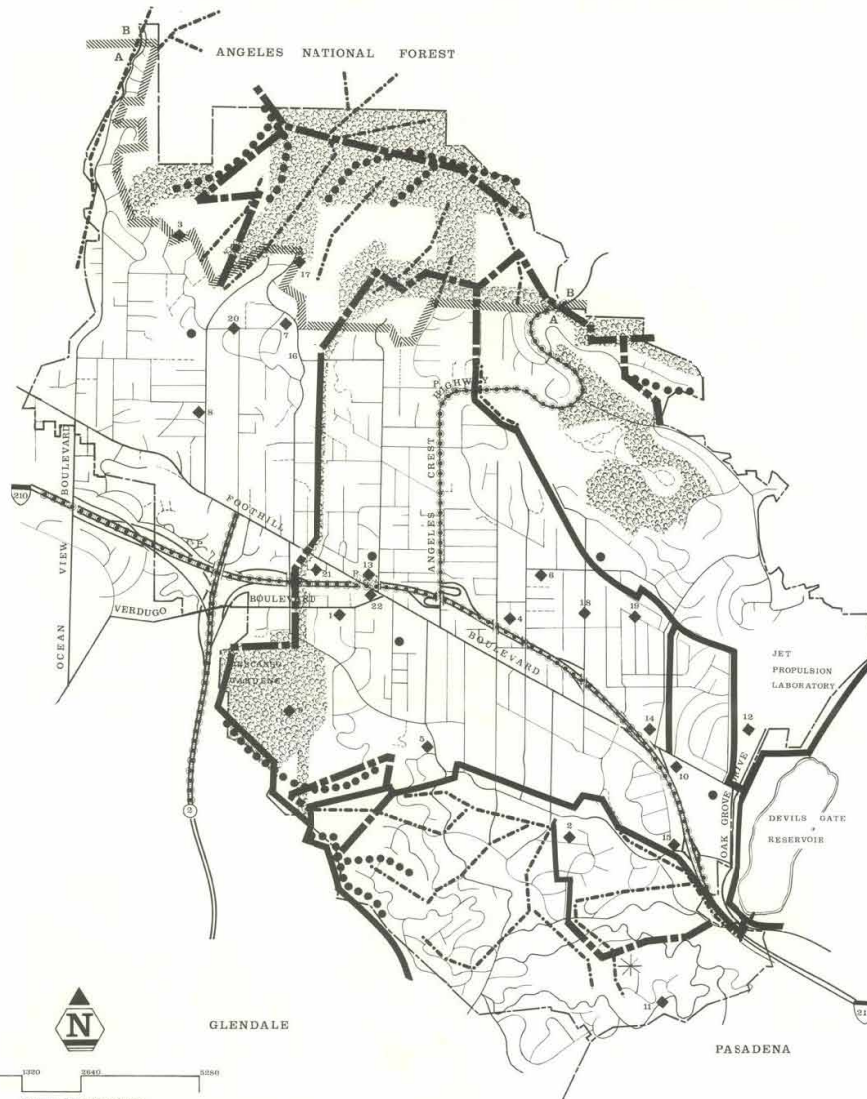
AYES: COUNCILMEMBERS: Olhasso, Spence and Brown, Portantino
NOES: COUNCILMEMBERS: None
ABSENT: COUNCILMEMBERS: Del Guercio
ABSTAIN: COUNCILMEMBERS: None

Dated: April 6, 2006
Published: April 13, 2006

Kathleen R. Sessman, City Clerk

City of La Cañada Flintridge

ENVIRONMENTAL RESOURCES MANAGEMENT PLAN



Legend

NATURAL FEATURES

- Ridgeline
- * Knoll
- Watercourse

TRAILS

- Major
- Minor
- Proposed

- ◆ Historic Resource - number refers to the resource described in section 3.4, ERME
- ▨ Hillside Management Line (HML)
- B Overlay- properties located north of the HML consist of very steeply sloping hillsides. Residential densities may not exceed 1 unit per 5 acres.
- A Overlay- properties located south of the HML consist of moderately to steeply sloping hillsides. Residential densities may not exceed 1 unit per acre.
- Open Space
- Park
- Playground
- **** Scenic Corridor

THIS MAP GENERALLY ILLUSTRATES THE POLICIES PROPOSED BY THE LA CAÑADA FLINTRIDGE GENERAL PLAN. THE SPECIFIC LOCATION OF BOUNDARIES ARE SHOWN ON A MAP, WITH A SCALE OF ONE INCH TO 500 FEET, IN THE LA CAÑADA FLINTRIDGE CITY OFFICE. FOR AN EXPLANATION OF THIS MAP, REFER TO THE LA CAÑADA FLINTRIDGE GENERAL PLAN — ENVIRONMENTAL RESOURCES MANAGEMENT ELEMENT.

PASSED AND APPROVED BY THE CITY OF
LA CAÑADA FLINTRIDGE CITY PLANNING
COMMISSION BY RESOLUTION NUMBER
PC 78-21 ON OCTOBER 2, 1975.

Arthur J. Morrison
CHAIRMAN

Ray H. Wynn
SECRETARY

PASSED AND ADOPTED BY THE CITY OF
LA CAÑADA FLINTRIDGE CITY COUNCIL
BY RESOLUTION NUMBER 80-10 ON
MARCH 3, 1980.

Richard J. Kelly
MAYOR

Carol L. Smithson
CITY CLERK

DEFENSE AND INDEMNIFICATION

State Codes and City Resolution

California Government Code Section 830-831.9.

831.4. A public entity, public employee, or a grantor of a public easement to a public entity for any of the following purposes, is not liable for an injury caused by a condition of:

(a) Any unpaved road which provides access to fishing, hunting, camping, hiking, riding, including animal and all types of vehicular riding, water sports, recreational or scenic areas and which is not a

(1) City street or highway or

(2) County, state or federal highway or

(3) Public street or highway of a joint highway district, boulevard district, bridge and highway district or similar district formed for the improvement or building of public streets or highways.

(b) Any trail used for the above purposes.

(c) Any paved trail, walkway, path, or sidewalk on an easement of way which has been granted to a public entity, which easement provides access to any unimproved property, so long as such public entity shall reasonably attempt to provide adequate warnings of the existence of any condition of the paved trail, walkway, path, or sidewalk which constitutes a hazard to health or safety. Warnings required by this subdivision shall only be required where pathways are paved, and such requirement shall not be construed to be a standard of care for any unpaved pathways or roads.

California Civil Code 846.

An owner of any estate or any other interest in real property, whether possessory or non-possessory, owes no duty of care to keep the premises safe for entry or use by others for any recreational purpose or to give any warning of hazardous conditions, uses of, structures, or activities on such premises to persons entering for such purpose, except as provided in this section. A "recreational purpose," as used in this section, includes such activities as fishing, hunting, camping, water sports, hiking, spelunking, sport parachuting, riding, including animal riding, snowmobiling, and all other types of vehicular riding, rock collecting, sightseeing, picnicking, nature study, nature contacting, recreational gardening, gleaning, hang gliding, winter sports, and viewing or enjoying historical, archaeological, scenic, natural, or scientific sites.

An owner of any estate or any other interest in real property, whether possessory or non-possessory, who gives permission to another for entry or use for the above purpose upon the premises does not thereby (a) extend any assurance that the premises are safe for such purpose, or (b) constitute the person to whom permission has been granted the legal status of an invitee or licensee to whom a duty of care is owed, or (c) assume responsibility for or incur liability for any injury to person or property caused by any act of such person to whom permission has been granted except as provided in this section.

This section does not limit the liability which otherwise exists (a) for willful or malicious failure to guard or warn against a dangerous condition, use, structure or activity; or (b) for injury suffered in any case where permission to enter for the above purpose as granted for a consideration other than

the consideration, if any, paid to said landowner by the state, or where consideration has been received from others for the same purpose; or (c) to any persons who are expressly invited rather than merely permitted to come upon the premises by the landowner.

Nothing in this section creates a duty of care or ground of liability for injury to person or property.

California Civil Code 846.1.

(a) Except as provided in subdivision (c), an owner of any estate or interest in real property, whether possessory or nonpossessory, who gives permission to the public for entry on or use of the real property pursuant to an agreement with a public or nonprofit agency for purposes of recreational trail use, and is a defendant in a civil action brought by, or on behalf of, a person who is allegedly injured or allegedly suffers damages on the real property, may present a claim to the State Board of Control for reasonable attorney's fees incurred in this civil action if any of the following occurs:

- (1) The court has dismissed the civil action upon a demurrer or motion for summary judgment made by the owner or upon its own motion for lack of prosecution.
- (2) The action was dismissed by the plaintiff without any payment from the owner.
- (3) The owner prevails in the civil action.

(b) Except as provided in subdivision (c), a public entity, as defined in Section 831.5 of the Government Code, that gives permission to the public for entry on or use of real property for a recreational purpose, as defined in Section 846, and is a defendant in a civil action brought by, or on behalf of, a person who is allegedly injured or allegedly suffers damages on the real property, may present a claim to the State Board of Control for reasonable attorney's fees incurred in this civil action if any of the following occurs:

- (1) The court has dismissed the civil action upon a demurrer or motion for summary judgment made by this public entity or upon its own motion for lack of prosecution.
- (2) The action was dismissed by the plaintiff without any payment from the public entity.
- (3) The public entity prevails in the civil action.

(c) An owner of any estate or interest in real property, whether possessory or non-possessory, or a public entity, as defined in Section 831.5 of the Government Code, that gives permission to the public for entry on, or use of, the real property for a recreational purpose, as defined in Section 846, pursuant to an agreement with a public or nonprofit agency, and is a defendant in a civil action brought by, or on behalf of, a person who seeks to restrict, prevent, or delay public use of that property, may present a claim to the State Board of Control for reasonable attorney's fees incurred in the civil action if any of the following occurs:

- (1) The court has dismissed the civil action upon a demurrer or motion for summary judgment made by the owner or public entity or upon its own motion for lack of prosecution.
- (2) The action was dismissed by the plaintiff without any payment from the owner or public entity.
- (3) The owner or public entity prevails in the civil action.

(d) The State Board of Control shall allow the claim if the requirements of this section are met. The claim shall be paid from an appropriation to be made for that purpose. Reasonable attorneys' fees, for purposes of this section, may not exceed an hourly rate greater than the rate charged by the Attorney General at the time the award is made, and may not exceed an aggregate amount of twenty-five thousand dollars (\$25,000). This subdivision shall not apply if a public entity has provided for the defense of this civil action pursuant to Section 995 of the Government Code. This subdivision shall also not apply if an owner or public

entity has been provided a legal defense by the state pursuant to any contract or other legal obligation.

(e) The total of claims allowed by the board pursuant to this section shall not exceed two hundred thousand dollars (\$200,000) per fiscal year.

City Resolution 04-50

Resolution of the City Council of the City of La Cañada Flintridge, Authorizing the City Manager to Determine the Provision of Workers' Compensation Coverage for Certain City Volunteers Pursuant to the Provisions of Section 3363.5 of the Labor Code.

WHEREAS, the City Council of the City of La Cañada Flintridge finds that it is in its best interests to utilize volunteers in the provision of certain governmental services; and

WHEREAS, said volunteers should be provided workers' compensation coverage while on duty in order to protect the volunteer while providing services to the City and to encourage volunteerism.

NOW, THEREFORE, the City Council of the City of La Cañada Flintridge does hereby resolve as follows:

1. The public interest is best served by providing workers' compensation coverage for City volunteer workers.
2. The City Manager is authorized to use his/her discretion to determine when volunteers are providing volunteer services for the City and to promulgate administrative procedures to track volunteers' services.
3. The eligibility for said volunteers for workers' compensation benefits will be applicable during the time the person actually performs volunteer services, provided, however, that the rights of the volunteers shall be limited as set forth in the Labor Code.

PASSED, APPROVED AND ADOPTED THIS 4TH DAY OF OCTOBER, 2004.



CITY OF LA CAÑADA FLINTRIDGE

PUBLIC WORKS DEPARTMENT

1327 Foothill Boulevard, La Cañada Flintridge, CA 91011 • 818-790-8882

RECREATIONAL TRAIL INSPECTION REPORT

THE FOLLOWING CONDITION EXISTS ON A TRAIL AS OF _____.
(DATE CONDITION OBSERVED)

TRAIL NAME (IF KNOWN): _____

NEAREST STREET ADDRESS TO TRAIL OR OTHER DESCRIPTION OF EXACT LOCATION:

LA CAÑADA FLINTRIDGE, CA 91011

REPORT SUBMITTED BY:

NAME: _____

ADDRESS: _____

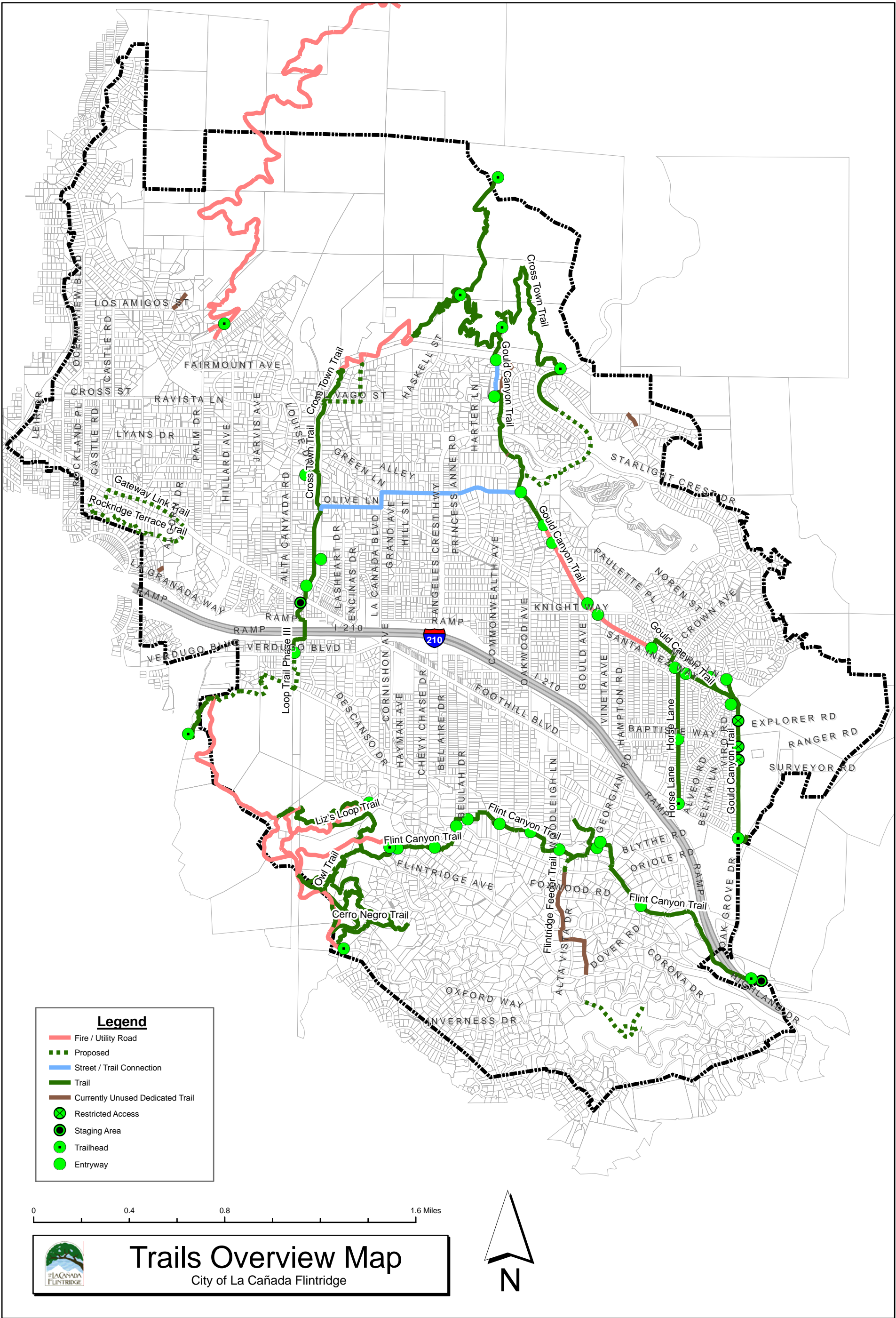
CITY: _____

TELEPHONE NUMBER: _____

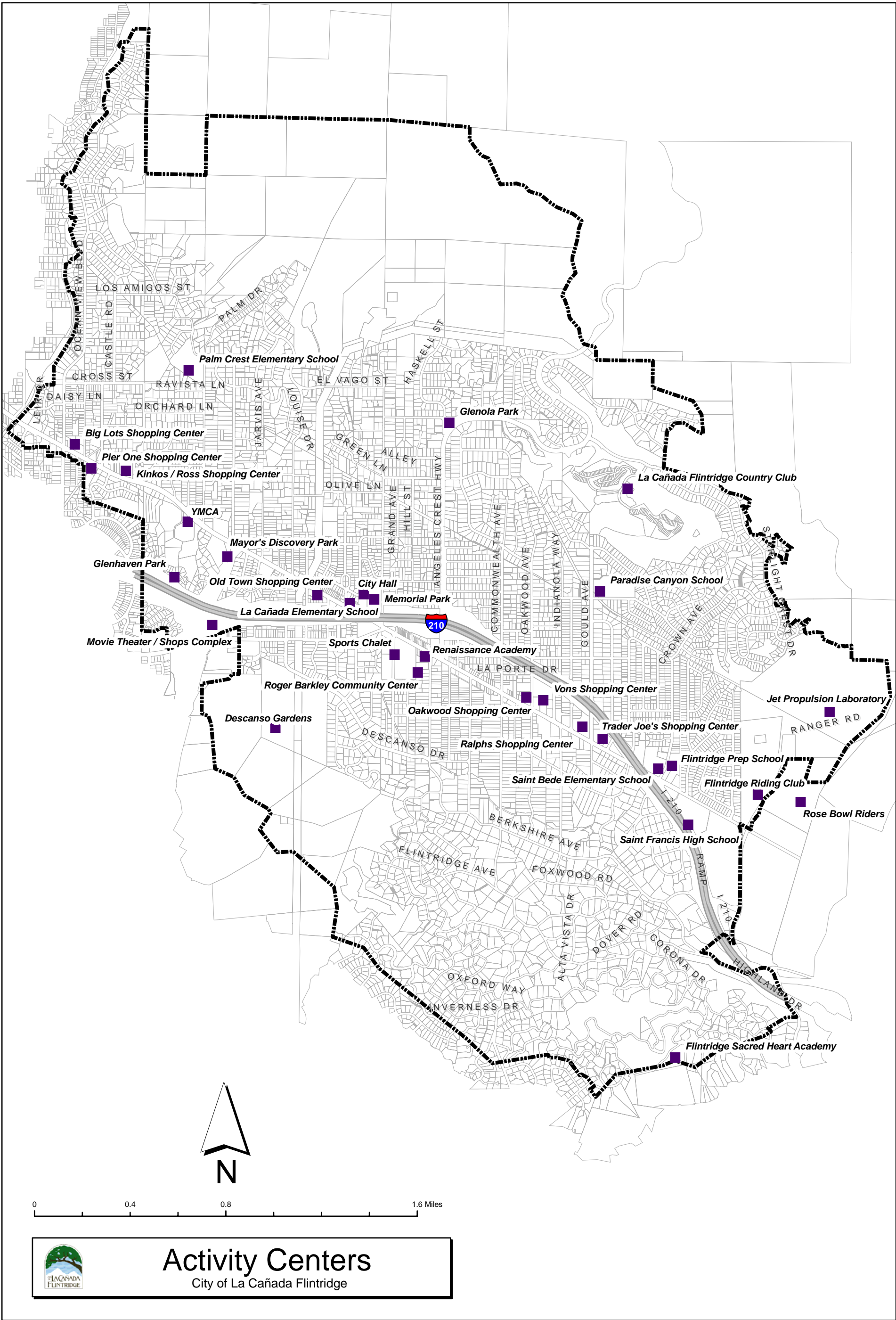
DATE OF SUBMISSION OF REPORT: _____

PUBLIC WORKS DEPARTMENT FOLLOW-UP:

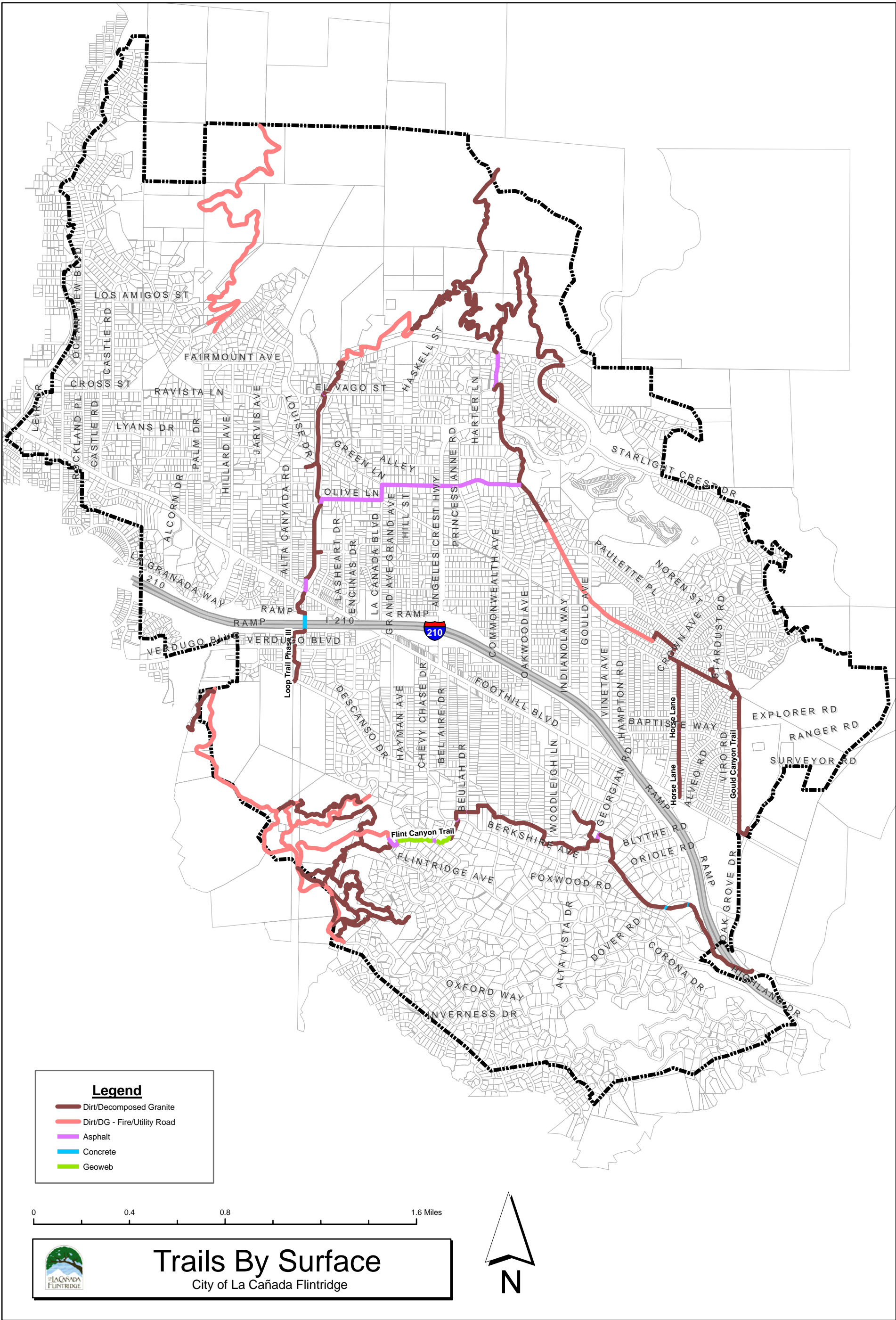
TRAILS OVERVIEW MAP



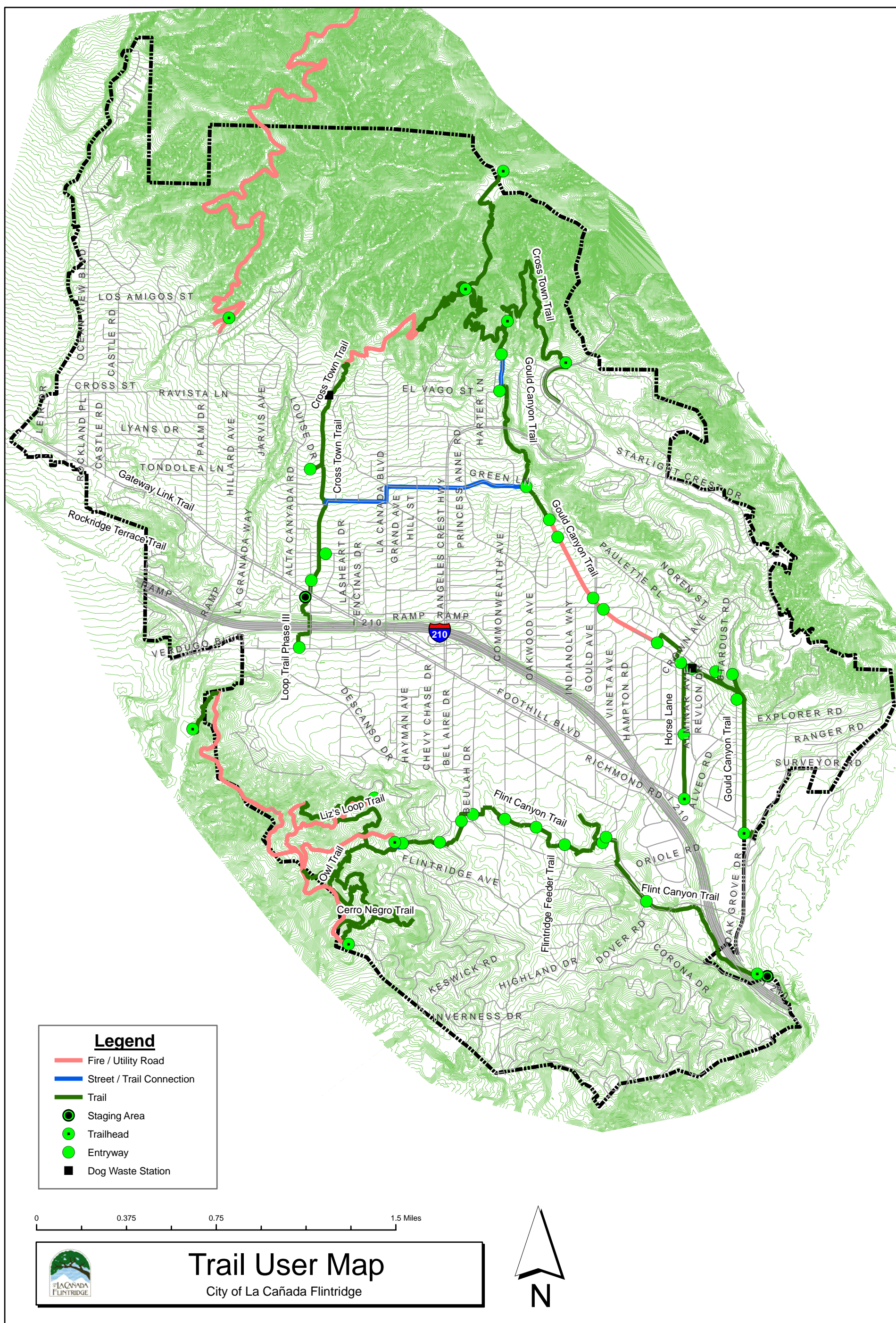
ACTIVITY CENTER MAP



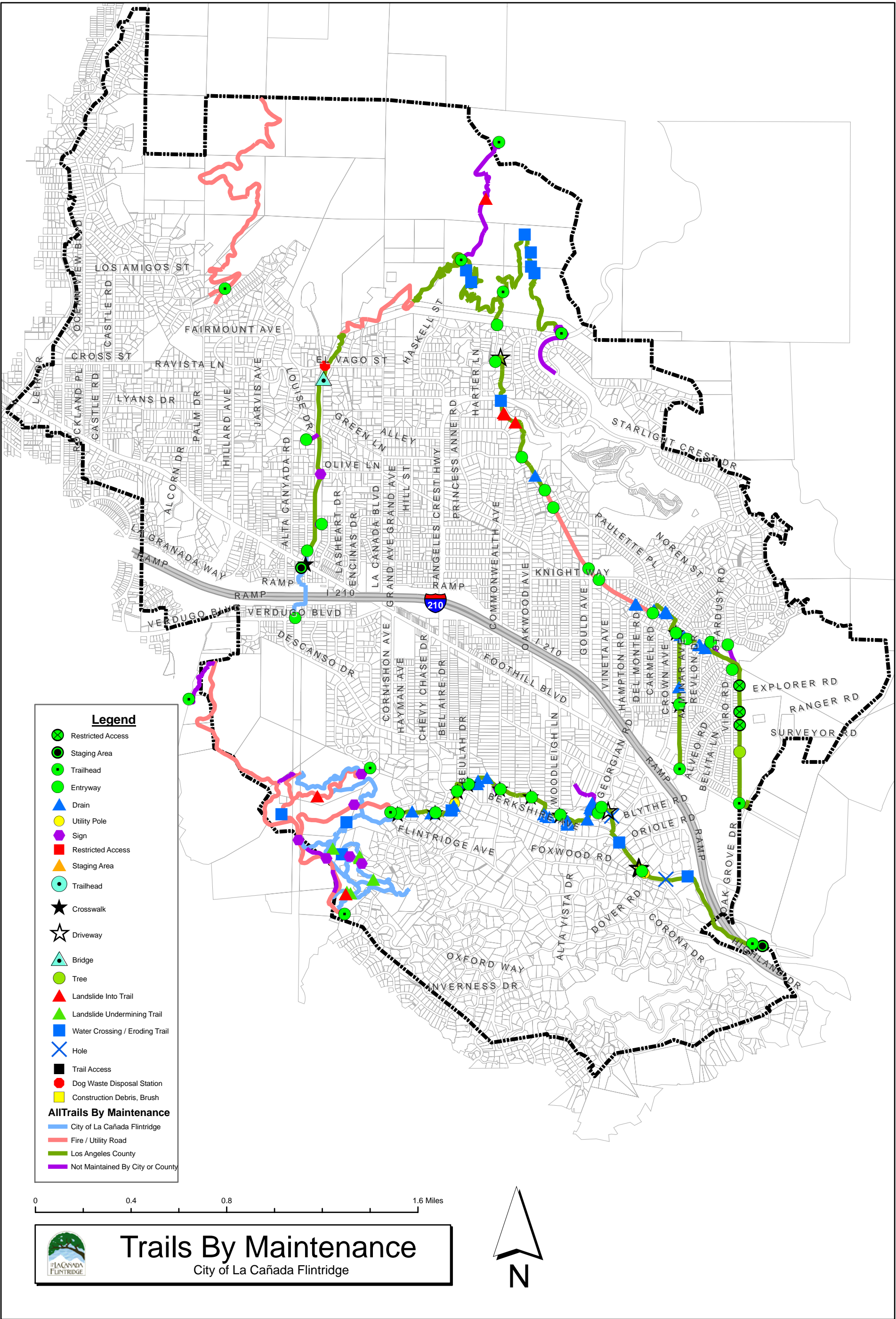
TRAILS BY SURFACE MAP



TRAIL USER MAP



TRAILS BY MAINTENANCE MAP



TRAILS-AFFECTED PROPERTIES MAP

